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[illegible]

	PLAN
	REFERENCE
	NO.
	SHEET
	OF
	SHEETS

NOTE: ALL SHEET REFERENCES, FIRST NOS. OF STRUCTURE CODE DESIGNATIONS
AND MATCH LINE SHEET REFERENCES, ETC., THROUGHOUT THE PLANS,
REFER TO THE ENTRY IN THE PLAN REFERENCE NUMBER BOX.

FILE NAME \\Wsdot\loc\sw\Group\Engineering\444301\05-Design\02-Projects\I-5 NB & SB Lewis Rlver Br and NB Toutle Rlver Br Deck Repair\1-CADD-Plans\ContractPlans\001_22X333_PS_IN.dgn										Plot 1															
TIME 2:05:59 PM								REGION NO.		STATE		FED.AID PROJ.NO. NHFP-0051(316)		<div><div></div><div>Washington State Department of Transportation</div></div> <div>I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY</div>										PLAN REF. NO.	
DATE 4/25/2023								10		WASH														IN2	
PLOTTED BY hillb								JOB NUMBER		22X333														SHEET 5 OF 84 SHEETS	
DESIGNED BY T. PANKAU								CONTRACT NO.		LOCATION NO.		XL6379, XL6380		<div><div></div><div>Washington State Department of Transportation</div></div> <div>INDEX</div>											
ENTERED BY CCAEO																									
CHECKED BY A. ROMANCHOCK																									
PROJ. ENGR. S. FELL																									
REGIONAL ADM. C. FRANCIS				REVISION		DATE		BY																	


PROJECT LICENSED PROFESSIONAL CERTIFICATES

<div></div> <div>Susan Fell</div> <div>May 2, 2023</div> <div>AS A LICENSED PROFESSIONAL IN DIRECT RESPONSIBLE CHARGE OF DEVELOPING THIS CONTRACT, I CERTIFY THAT ALL PLANS THAT CONTAIN MY STAMP HAVE BEEN DEVELOPED UNDER MY SUPERVISION.</div>	<div></div> <div>LisaRene Schilperoort</div> <div>May 2, 2023</div> <div>AS A LICENSED PROFESSIONAL IN DIRECT RESPONSIBLE CHARGE OF DEVELOPING THIS CONTRACT, I CERTIFY THAT ALL PLANS THAT CONTAIN MY STAMP HAVE BEEN DEVELOPED UNDER MY SUPERVISION.</div>	<div></div> <div>Geoffrey D Swett</div> <div>May 2, 2023</div> <div>AS A LICENSED PROFESSIONAL IN DIRECT RESPONSIBLE CHARGE OF DEVELOPING THIS CONTRACT, I CERTIFY THAT ALL PLANS THAT CONTAIN MY STAMP HAVE BEEN DEVELOPED UNDER MY SUPERVISION.</div>	<div></div> <div>Richard Zeldenrust</div> <div>May 2, 2023</div> <div>AS A LICENSED PROFESSIONAL IN DIRECT RESPONSIBLE CHARGE OF DEVELOPING THIS CONTRACT, I CERTIFY THAT ALL PLANS THAT CONTAIN MY STAMP HAVE BEEN DEVELOPED UNDER MY SUPERVISION.</div>
<div></div> <div>Genmiao Chen (May 2, 2023 08:13 PDT)</div> <div>Genmiao Chen</div> <div>May 2, 2023</div> <div>AS A LICENSED PROFESSIONAL IN DIRECT RESPONSIBLE CHARGE OF DEVELOPING THIS CONTRACT, I CERTIFY THAT ALL PLANS THAT CONTAIN MY STAMP HAVE BEEN DEVELOPED UNDER MY SUPERVISION.</div>			

NOTES:

THIS PLAN SET WAS DEVELOPED ELECTRONICALLY UNDER THE DIRECT SUPERVISION OF THE LICENSED PROFESSIONALS THAT HAVE AFFIXED THEIR SIGNATURE TO THIS PAGE.

THIS SHEET SERVES AS THE CERTIFICATION BY THE ABOVE LICENSED PROFESSIONALS OF ALL SHEETS IN THIS PLAN SET WHERE THEIR STAMPS AND SIGNATURES APPEAR.

FILE NAME										\\Wsdot.loc\sw\Group\Engineering\444301\05-Design\02-Projects\I-5 NB & SB Lewis River Br and NB Toutle River Br Deck Repair\1-CADD-Plans\ContractPlans\002_22X333_PS_CT.dgn												I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY		PLAN REF NO	
TIME		2:12:09 PM								REGION NO.		STATE		FED.AID PROJ.NO.		CT1									
DATE		3/1/2023								10		WASH		NHFP-0051(316)				SHEET 6 OF 84 SHEETS							
PLOTTED BY		hillb																							
DESIGNED BY		T. PANKAU										JOB NUMBER		22X333											
ENTERED BY		CLARK COUNTY AEO												CONTRACT NO.		LOCATION NO.									
CHECKED BY		A. ROMANCHOCK														XL6379, XL6380									
PROJ. ENGR.		S. FELL																							
REGIONAL ADM.		C. FRANCIS																							
				REVISION				DATE		BY															

QUANTITY TABULATION - STAGING

[illegible]

QUANTITY TABULATION - STAGING

NOTE:
THE FIRST NUMBER OF THE "CODE" BELOW
REFERS TO THE SHEET NO. OR THE SHEET
REFERENCE NO. SHOWING THE
CONSTRUCTION FEATURE.

THE SECOND NUMBER REFERS TO THE CONSTRUCTION FEATURE FOUND ON THAT SHEET.

GENERAL NOTES:

CODE	LOCATION ▼ \ UNIT OF MEASURE ►
------	--------------------------------

	**** STAGE B QUANTITIES ****
--	------------------------------

SU10-1 | N 73+33 TO 94+51 (36' RT)

SU10-2 | N 73+33 TO 94+51 (24' RT)

SU10-3 | N 73+33 TO 94+51 (12' RT)

SU11-1 BRIDGE TRANSITION - SOUTH APPROACH

SU11-2 | BRIDGE TRANSITION - NORTH APPROACH

SU11-3 | N 79+94 TO 92+76 (8' RT)

SU22-1	S 78+88 TO 101+11 (12' LT)
--------	----------------------------

SU22-2 | S 78+88 TO 101+11 (24' LT)

SU22-3 | S 78+88 TO 101+11 (36' LT)

SU22-4 | S 78+88 TO 94+50 (8' LT)

SU22-5 | BRIDGE TRANSITION - SOUTH APPROACH

SHEET TOTAL	
PROJECT TOTAL	

DESIGNED BY
ENTERED BY
CHECKED BY
PROJ. ENGR.
REGION ADM.

T. PANKAU
CCEO
A. ROMANCHOCK
S. FELL
C. FRANCIS

DATE _____

DATE _____

REVISION

BY

REGION NO.	
------------	--

10

STATE

WASH

JOB NUMBER	22X333
CONTRACT NO.	

FED. AID PROJ. NO.	
--------------------	--

NHFP-0051(316)



**Washington State
Department of Transportation**

**I-5
NB AND SB LEWIS RVR BR
DECK REPAIR / OVERLAY**

QUANTITY TABULATION - STAGING

QTSU 2

SHEET
8
OF
84
SHEETS

ALL SIGNS ARE 36"x48" BLACK ON ORANGE UNLESS OTHERWISE INDICATED

SIGNS SHOWN WITHIN THESE STAGING PLANS SHALL BE CLASS A AND POST-MOUNTED.

CONFLICTING SIGNS SHALL BE COVERED IN ACCORDANCE WITH STANDARD SPEC. 8-21.3(3) (SIGN COVERING WILL BE PAID UNDER OTHER ITEMS OF WORK).

THESE STAGING PLANS ARE FOR GENERAL INFORMATION ONLY AND DO NOT SHOW EVERY STAGE OR CONSTRUCTION ACTIVITY THAT THE CONTRACTOR MAY NEED TO COMPLETE THE PROJECT. WHERE NEEDED, THE CONTRACTOR SHALL DEVISE THEIR OWN STAGING PLANS. PRIOR APPROVAL IS REQUIRED FOR NEW STAGING PLANS, OR FOR MODIFICATIONS TO THIS ONE.

THE CONTRACTOR SHALL HAVE AN ADDITIONAL 3 PCMS BOARDS FOR ADVANCE NOTIFICATION ON LOCAL ROADS DURING EACH EXTENDED CLOSURE. PLACEMENT OF THE PCMS BOARDS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

SB I-5

NB I-5 (BRIDGE #05/40E)

40 45 50

N STA 40+00
MP 19.07
GATED W4-2L

48"
48"
W4-2L

NW PACIFIC HWY

N STA 47+20
MP 19.21
BEGIN SHOULDER TAPER (48' RT) (20:1)

N STA 49+00
MP 19.24
SWZS PCMS1 & TRAFFIC SENSOR A
PAN-TILT-ZOOM CAMERA
SEE SWZS TABLE ON SHEET SU1

N STA 49+50
MP 19.26
SEQUENTIAL
ARROWBOARD



N STA 49+63
MP 19.26
END SHOULDER TAPER (20:1) (36' RT)
BEGIN MERGING TAPER (60:1) (36' RT)

12'
12'
12'

PCMS1

MATCH LINE 1 - N STA 51+00

0 50 100
SCALE IN FEET

FILE NAME \\Wsdot\locswl\Group\Engineering\444301\05-Design\02-Projects\I-5 NB & SB Lewis River Br and NB Toutle River Br Deck Repair\1-CADD-Plans_ContractPlans\008-027_22X333_PS_SJA.dgn										Plot 2	
TIME	1:21:21 PM				REGION NO.	STATE	FED.AID PROJ.NO.		PLAN REF NO		
DATE	3/16/2023				10	WASH	NHFP-0051(316)		SU2		
PLOTTED BY	hillb				JOB NUMBER						
DESIGNED BY	T. PANKAU				22X333						
ENTERED BY	CCEO				CONTRACT NO.						
CHECKED BY	A. ROMANCHOCK						LOCATION NO.		SHEET 10 OF 84 SHEETS		
PROJ. ENGR.	S. FELL						XL6379	SEE SHEET CT1 DATE _____	DATE _____		
REGIONAL ADM.	C. FRANCIS	REVISION	DATE	BY	P.E. STAMP BOX _____						
<div> Washington State Department of Transportation</div>										STAGING PLAN - BRIDGE 5/40E STAGE A	

ALL SIGNS ARE 28"x48" BLACK ON ORANGE UNLESS OTHERWISE INDICATED

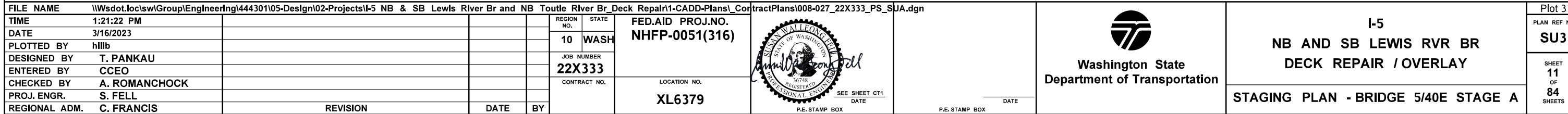
SIGNS SHOWN WITHIN THESE STAGING PLANS SHALL BE CLASS A AND POST-MOUNTED.

CONFLICTING SIGNS SHALL BE COVERED IN ACCORDANCE WITH STANDARD SPEC. 8-21.3(3) (SIGN COVERING WILL BE PAID UNDER OTHER ITEMS OF WORK).

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EQUATION SB I-5 STATION EQUATION
MP 19.45 BACK
MP 19.39 AHEAD



----- EXISTING EDGE LINE

----- EXISTING MEDIAN EDGE LINE

----- EXISTING OUTSIDE EDGE LINE

----- EXISTING LANE LINE

----- EXISTING PAVED SHOULDER

----- EXISTING RUMBLE STRIP

----- EXISTING CABLE BARRIER

----- EXISTING BARRIER

----- EXISTING GUARD RAIL

19 MILEPOST

?? ?? QUANTITY NOTE

LEGEND

STATION ALIGNMENT

TEMPORARY PAVEMENT MARKING

TEMPORARY BARRIER

IMPACT ATTENUATOR

SHOULDER REPAIR

TRAFFIC MOVEMENT ARROW

HMA TAPER LIMITS

28" REFLECTIVE TRAFFIC CONE

PAN-TILT-ZOOM CAMERA

SFTS SIDE-FIRE TRAFFIC SENSOR

⊗ TRAFFIC SAFETY DRUM

➤ SIGN LOCATION

➤ TRAFFIC SENSOR

➤ SEQUENTIAL ARROW SIGN

PCMS PORTABLE CHANGEABLE MESSAGE SIGN

TTR# PORTABLE TRAVEL TIME READER

TEMPORARY SIGN LOCATION (5' MIN HEIGHT)

RADAR SPEED DISPLAY SIGN

ALL SIGNS ARE 48"x48" BLACK ON ORANGE UNLESS OTHERWISE INDICATED

SIGNS SHOWN WITHIN THESE STAGING PLANS SHALL BE CLASS A AND POST-MOUNTED.

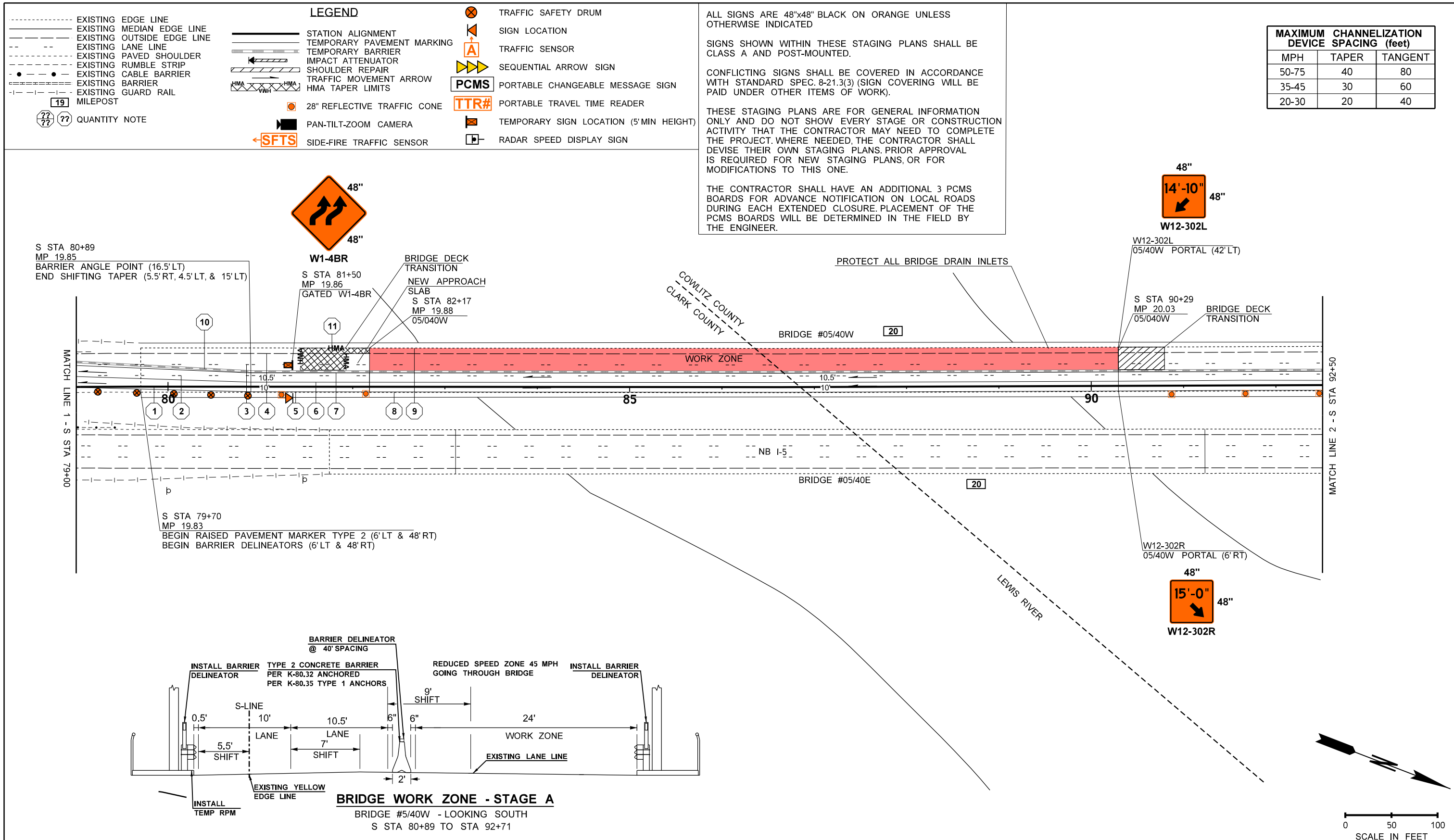
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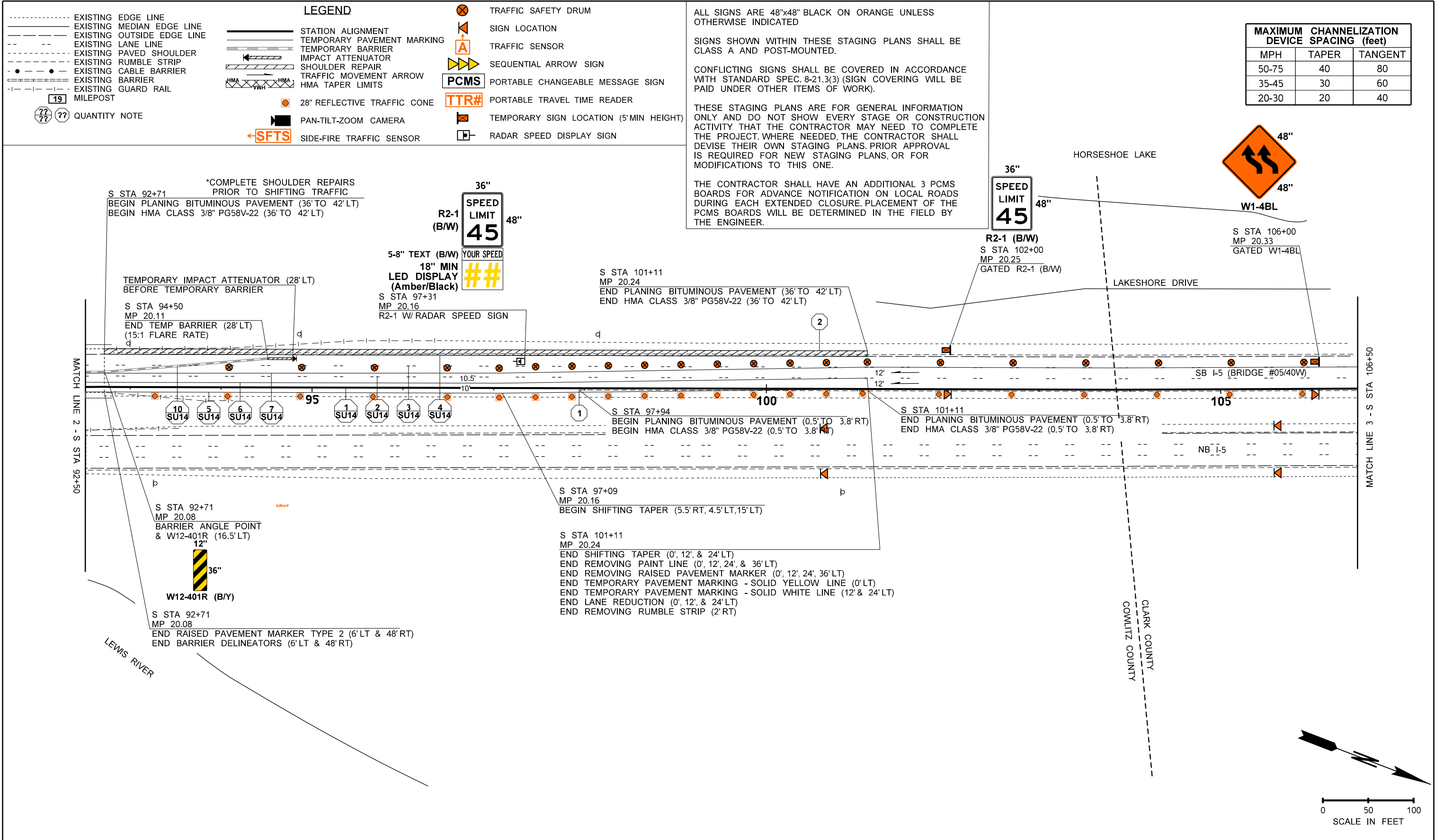
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MAXIMUM CHANNELIZATION DEVICE SPACING (feet)		
MPH	TAPER	TANGENT
50-75	40	80
35-45	30	60
20-30	20	40

FILE NAME \\Wsdot\loc\sw\Group\Engineering\444301\05-Design\02-Projects\I-5 NB & SB Lewis River Br and NB Toutle River Br Deck Repair\1-CADD-Plans\ContractPlans\014-035_22X333_PS_SJB.dgn				FED.AID PROJ.NO. NHFP-0051(316)			Washington State Department of Transportation	I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY	Plot 2						
TIME 1:37:25 PM	DATE 3/16/2023	PLOTTED BY hillb	PLAN REF NO SU8												
DESIGNED BY T. PANKAU	ENTERED BY CCEO	CHECKED BY A. ROMANCHOCK	PROJ. ENGR. S. FELL	REGIONAL ADM. C. FRANCIS	REVISION	DATE	BY	JOB NUMBER 22X333	CONTRACT NO.	LOCATION NO. XL6379	SEE SHEET CT1 DATE	P.E. STAMP BOX	DATE	P.E. STAMP BOX	SHEET 16 OF 84 SHEETS



FILE NAME \\Wsdot\locsw\Group\Engineering\444301\05-Design\02-Projects\I-5 NB & SB Lewis River Br and NB Toutle River Br Deck Repair\1-CADD-Plans\Contract\Plans\008-027_22X333_PS_SJA.dgn												Plot 12					
TIME 1:21:28 PM						REGION NO. 10		STATE WASH		FED.AID PROJ.NO. NHFP-0051(316)		PLAN REF NO SU14					
DATE 3/16/2023						JOB NUMBER 22X333						 Washington State Department of Transportation		I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY		SHEET 22 OF 84 SHEETS	
PLOTTED BY hillb						CONTRACT NO.		LOCATION NO. XL6380									
DESIGNED BY T. PANKAU																	
ENTERED BY CCEO																	
CHECKED BY A. ROMANCHOCK																	
PROJ. ENGR. S. FELL																	
REGIONAL ADM. C. FRANCIS		REVISION		DATE		BY											



----- EXISTING EDGE LINE

----- EXISTING MEDIAN EDGE LINE

----- EXISTING OUTSIDE EDGE LINE

----- EXISTING LANE LINE

----- EXISTING PAVED SHOULDER

----- EXISTING RUMBLE STRIP

----- EXISTING CABLE BARRIER

----- EXISTING BARRIER

----- EXISTING GUARD RAIL

19 MILEPOST

?? ?? QUANTITY NOTE

LEGEND

STATION ALIGNMENT

TEMPORARY PAVEMENT MARKING

TEMPORARY BARRIER

IMPACT ATTENUATOR

SHOULDER REPAIR

TRAFFIC MOVEMENT ARROW

HMA TAPER LIMITS

28" REFLECTIVE TRAFFIC CONE

PAN-TILT-ZOOM CAMERA

SFTS SIDE-FIRE TRAFFIC SENSOR

TRAFFIC SAFETY DRUM

SIGN LOCATION

TRAFFIC SENSOR

SEQUENTIAL ARROW SIGN

PCMS PORTABLE CHANGEABLE MESSAGE SIGN

TTR# PORTABLE TRAVEL TIME READER

TEMPORARY SIGN LOCATION (5'MIN HEIGHT)

RADAR SPEED DISPLAY SIGN

MAXIMUM CHANNELIZATION DEVICE SPACING (feet)

MPH	TAPER	TANGENT
50-75	40	80
35-45	30	60
20-30	20	40

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SB I-5 (BRIDGE #05/40W)

110 115 120

12' 12'

PCMS

W5-1
S STA 112+00
MP 20.45
GATED W5-1

W3-5
S STA 115+84
MP 20.52
GATED W3-5

LAKESHORE DRIVE

20.5

20.38

PCMS		
1	2	3
CAUTION NARROW LANES	SPEED REDUCED 45 MPH	1/4 MILE AHEAD
1.5 SEC	1.5 SEC	1.5 SEC

LOCATE PCMS PER WSDOT STANDARD SPEC. 1-10.3(3)C.

WOODLAND STATE AIRPORT

FILE NAME \\Wsdot\loc\sw\Group\Engineering\444301\05-Design\02-Projects\I-5 NB & SB Lewis River Br and NB Toutle River Br Deck Repair\1-CADD-Plans\ContractPlans\008-027_22X333_PS_SJA.dgn

TIME 1:21:31 PM

DATE 3/16/2023

PLOTTED BY hillb

DESIGNED BY T. PANKAU

ENTERED BY CCEO

CHECKED BY A. ROMANCHOCK

PROJ. ENGR. S. FELL

REGIONAL ADM. C. FRANCIS

REVISION

DATE

BY

REGION NO. 10

STATE WASH

JOB NUMBER 22X333

CONTRACT NO.

FED.AID PROJ.NO. NHFP-0051(316)

LOCATION NO. XL6380

SEE SHEET CT1

DATE

P.E. STAMP BOX

Washington State Department of Transportation

I-5

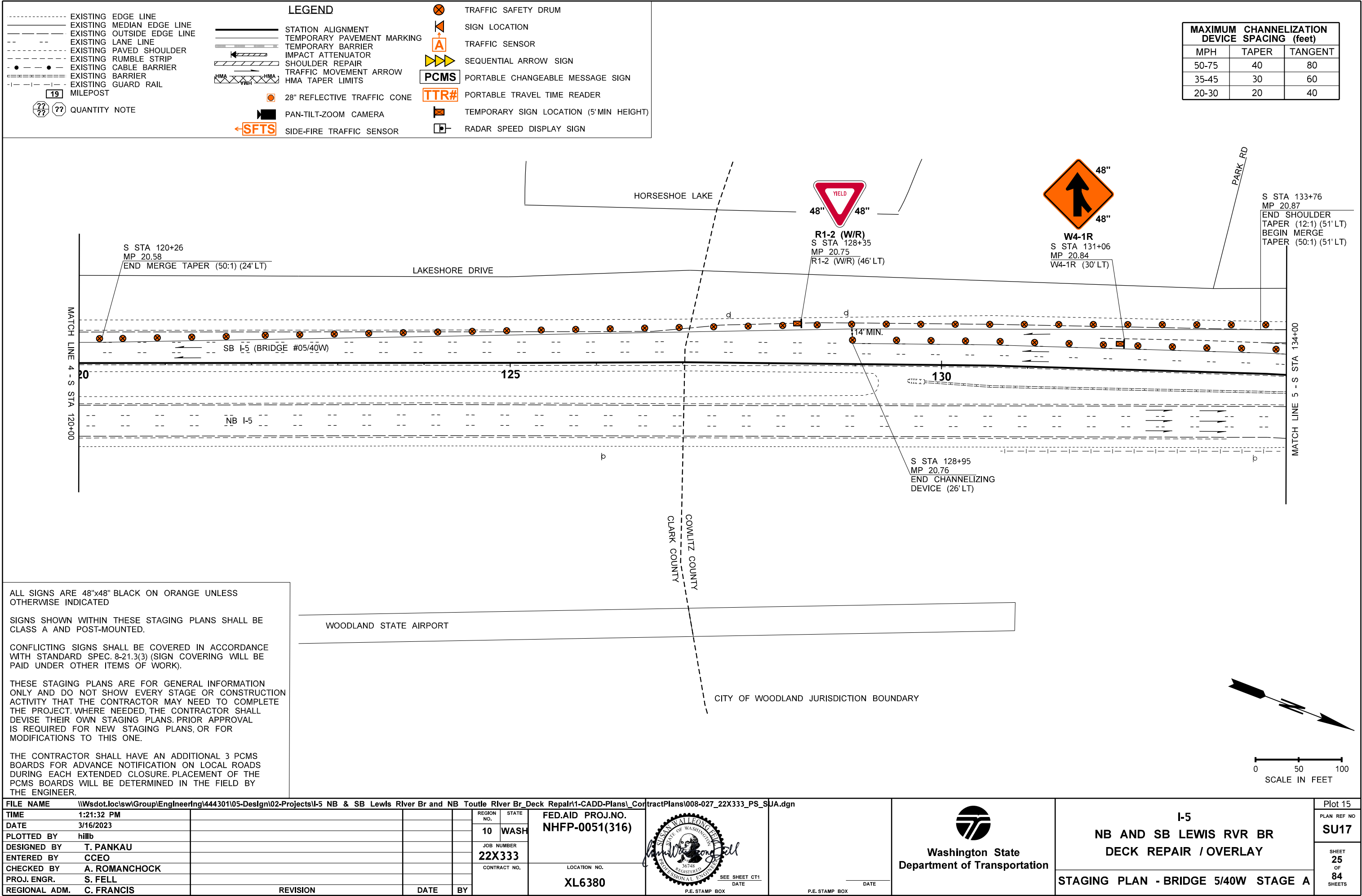
NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY

STAGING PLAN - BRIDGE 5/40W STAGE A

Plot 14

PLAN REF NO SU16

SHEET 24 OF 84 SHEETS



----- EXISTING EDGE LINE

----- EXISTING MEDIAN EDGE LINE

----- EXISTING OUTSIDE EDGE LINE

----- EXISTING LANE LINE

----- EXISTING PAVED SHOULDER

----- EXISTING RUMBLE STRIP

----- EXISTING CABLE BARRIER

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MAXIMUM CHANNELIZATION DEVICE SPACING (feet)

MPH	TAPER	TANGENT
50-75	40	80
35-45	30	60
20-30	20	40

NOTES:

1. THIS PLAN IS USED IN CONJUNCTION WITH APPLICABLE 2-LANE FREEWAY SINGLE RIGHT LANE CLOSURE TRAFFIC CONTROL PLAN (WITH PCMSs IN ADVANCE OF LANE CLOSURE TAPER REMOVED).

2. SEE SMART WORK ZONE SYSTEM (SWZS) SPECIAL PROVISION OR RFP FOR DETAILS.

3. MODIFICATIONS TO PCMS MESSAGES SHALL BE ACCEPTED BY THE ENGINEER. "##" ARE CHANGEABLE VALUES BASED ON REAL-TIME TRAVEL DELAY TIMES.

4. ADJUST SWZS COMPONENTS TO AVOID CONFLICTS WITH SEQUENTIAL ARROW SIGNS OR OTHER TRAFFIC CONTROL DEVICES, NARROW SHOULDERS, AND RAMPS.

5. LOCATE PCMSs PER STANDARD SPECIFICATION 1-10.3(3)C. PCMS MAY BE PLACED ON OPPOSITE SHOULDER BUT AVOID RAMP GORES. WHEN LOCATED BEHIND BARRIER/GUARDRAIL OR WITHIN CLOSURE, TRANSVERSE TRAFFIC DRUMS OPTIONAL.

6. MINITURE PCMS (~6' WIDE, 12+ INCH CHARACTERS) ALLOWED FOR PCMS1.

7. IN LIEU OF TRAVEL TIME READERS, ALTERNATIVE METHODS (SUCH AS USING TRAFFIC SENSOR SPEED DATA) IS ACCEPTABLE WHEN ACCURATE WITHIN 5+/- MINUTES.

8. IF SYSTEM FAILS SEE "SMART WORK ZONE SYSTEM FAILURE PROTOCOL" PROVISION.

9. IF TRAFFIC QUEUES REACH 8 MILES, PLACE ADDITIONAL PCMS AT 9.5 MILES. RELOCATE TO REMAIN 0.5+/- MILE IN ADVANCE OF QUEUE. TRUCK-MOUNTED PCMS WITH 10+ INCH CHARACTERS ACCEPTABLE. TRANSVERSE TRAFFIC SAFETY DRUMS OPTIONAL. REMOVE PCMS WHEN DISSIPATING QUEUES ARE LESS THAN 8 MILES.

PCMS MESSAGE: TRAFFIC BACKUPS PRESENT / WATCH FOR SLOW TRAFFIC

10. AVOID PCMS PLACEMENT ON HORIZONTAL CURVES.

36" SPEED LIMIT 60 48"

48" W3-5

GATED R2-1 (B/W) MP 21.49

GATED W3-5 MP 21.87

GATED W20-5R MP 21.68

GATED W20-1 MP 22.05

RIGHT LANE CLOSED AHEAD 48"

ROAD WORK AHEAD 48"

W20-5R

W20-1

SWZS PCMS 2 MP 22.10

SWZS PCMS 3 MP 23.15

SWZS PCMS 4 MP 24.10

SWZS PCMS 5 MP 25.60

SWZS PCMS 6 MP 27.10

SWZS PCMS 7 MP 28.60

SWZS PCMS 8 MP 30.10

TTR1 SEE NOTE 7

0.1 +/- MILE

9-MILE SMART WORK ZONE SYSTEM AND SIGNAGE FOR SB LEWIS BRIDGE (#05/40W)

QUEUE LOCATION (miles)	TRAFFIC SENSORS							PCMS 8		PCMS 7		PCMS 6		PCMS 5		PCMS 4		PCMS 3		PCMS 2		PCMS 1	
	H G F E D C B A							1	2	1	2	1	2	1	2	1	2	1	2	1	2	1	2
	TRAFFIC CONDITION							2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC	2.0 SEC
None	FF	FF	FF	FF	FF	FF	FF																
0.5 TO 1.4	FF	FF	FF	FF	FF	FF	SL																
1.41 TO 2.4	FF	FF	FF	FF	FF	SL	SL																
2.41 TO 3.4	FF	FF	FF	FF	SL	SL	SL																
3.41 TO 4.9	FF	FF	FF	SL	SL	SL	SL																
4.91 TO 6.4	FF	FF	SL	SL	SL	SL	SL																
6.41 TO 7.9	FF	SL	SL	SL	SL	SL	SL																
7.91+	SL	SL	SL	SL	SL	SL	SL																

FILE NAME \\Wsdot\loc\sw\Group\Engineering\444301\05-Design\02-Projects\I-5 NB & SB Lewis River Br and NB Toutle River Br Deck Repair\1-CADD-Plans\ContractPlans\008-027_22X333_PS_SJA.dgn

TIME 1:21:36 PM

DATE 3/16/2023

PLOTTED BY hillb

DESIGNED BY T. PANKAU

ENTERED BY CCEO

CHECKED BY A. ROMANCHOCK

PROJ. ENGR. S. FELL

REGIONAL ADM. C. FRANCIS

REGION NO. 10

STATE WASH

JOB NUMBER 22X333

CONTRACT NO.

REVISION

DATE

BY

FED.AID PROJ.NO. NHFP-0051(316)

LOCATION NO. XL6380

SEE SHEET CT1

DATE

P.E. STAMP BOX

Washington State Department of Transportation

DATE

P.E. STAMP BOX

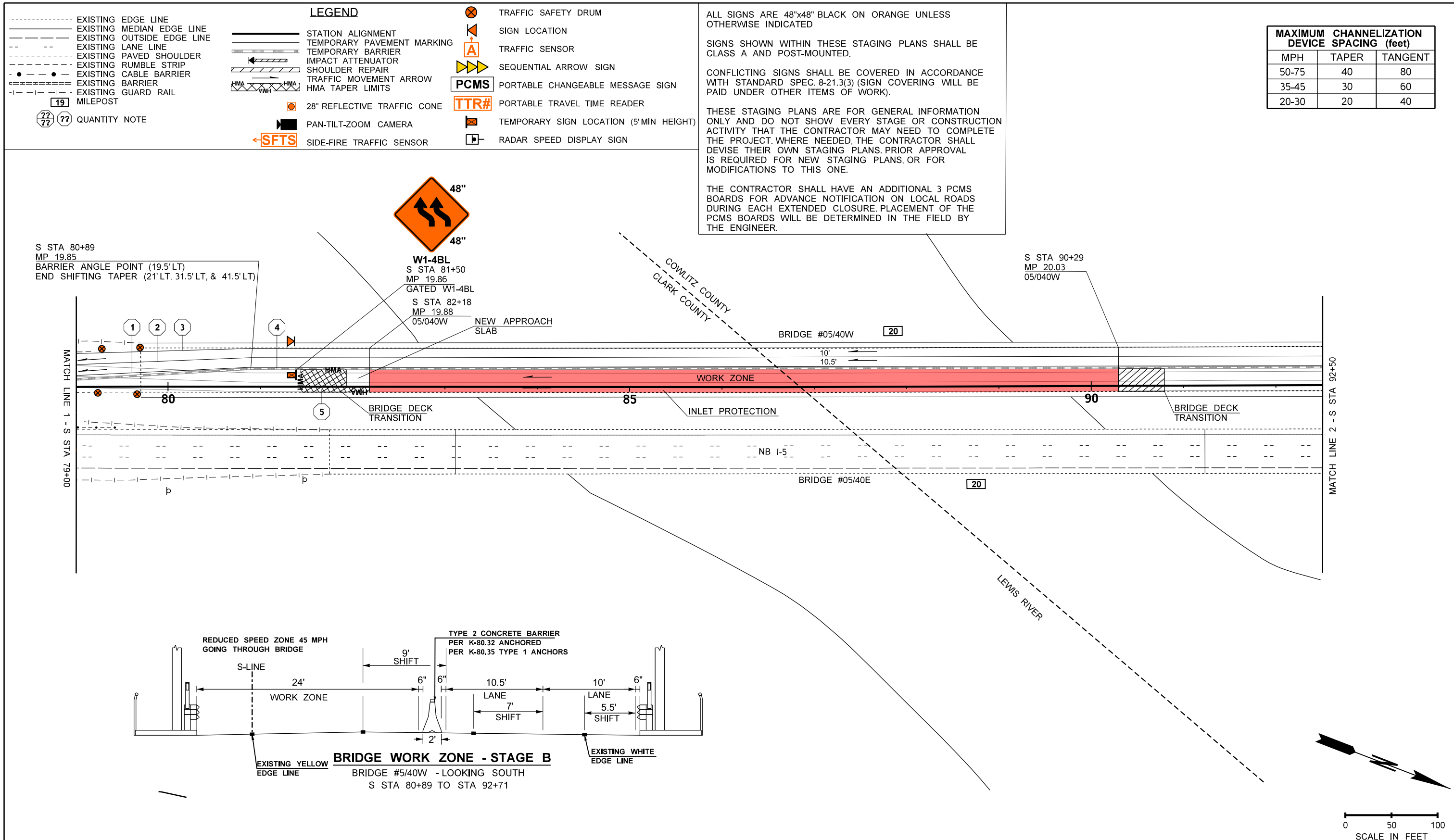
I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY




STAGING PLAN - BRIDGE 5/40W STAGE A













Plot 18

PLAN REF NO SU20

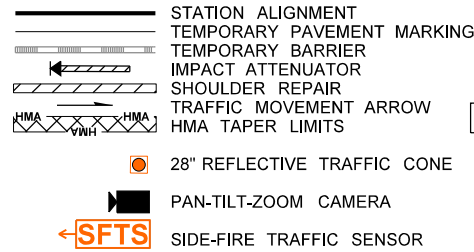
SHEET 28 OF 84 SHEETS











FILE NAME \\Wsdot\locsw\Group\Engineering\444301\05-Design\02-Projects\I-5 NB & SB Lewis River Br and NB Toutle River Br Deck Repair\1-CADD-Plans\Contract\Plans\014-035_22X333_PS_SUB.dgn												Plot 12						
TIME 1:37:34 PM							REGION NO. 10		STATE WASH		FED.AID PROJ.NO. NHFP-0051(316)		PLAN REF NO SU22					
DATE 3/16/2023							JOB NUMBER 22X333						 Washington State Department of Transportation		 I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY		SHEET 30 OF 84 SHEETS	
PLOTTED BY hillb							CONTRACT NO.		LOCATION NO. XL6380									
DESIGNED BY T. PANKAU																		
ENTERED BY CCEO																		
CHECKED BY A. ROMANCHOCK													STAGING PLAN - BRIDGE 5/40W STAGE B					
PROJ. ENGR. S. FELL																		
REGIONAL ADM. C. FRANCIS		REVISION			DATE		BY											

	EXISTING	EDGE LINE
	EXISTING	MEDIAN EDGE LINE
	EXISTING	OUTSIDE EDGE LINE
	EXISTING	LANE LINE
	EXISTING	PAVED SHOULDER
	EXISTING	RUMBLE STRIP
	EXISTING	CABLE BARRIER
	EXISTING	BARRIER
	EXISTING	GUARD RAIL
	19	MILEPOST
	??	QUANTITY NOTE
	??	

LEGEND



	TRAFFIC SAFETY DRUM
	SIGN LOCATION
	TRAFFIC SENSOR
	SEQUENTIAL ARROW SIGN
	PORTABLE CHANGEABLE MESSAGE SIGN
	PORTABLE TRAVEL TIME READER
	TEMPORARY SIGN LOCATION (5' MIN HEIGHT)
	RADAR SPEED DISPLAY SIGN

MAXIMUM CHANNELIZATION DEVICE SPACING (feet)		
MPH	TAPER	TANGENT
50-75	40	80
35-45	30	60
20-30	20	40

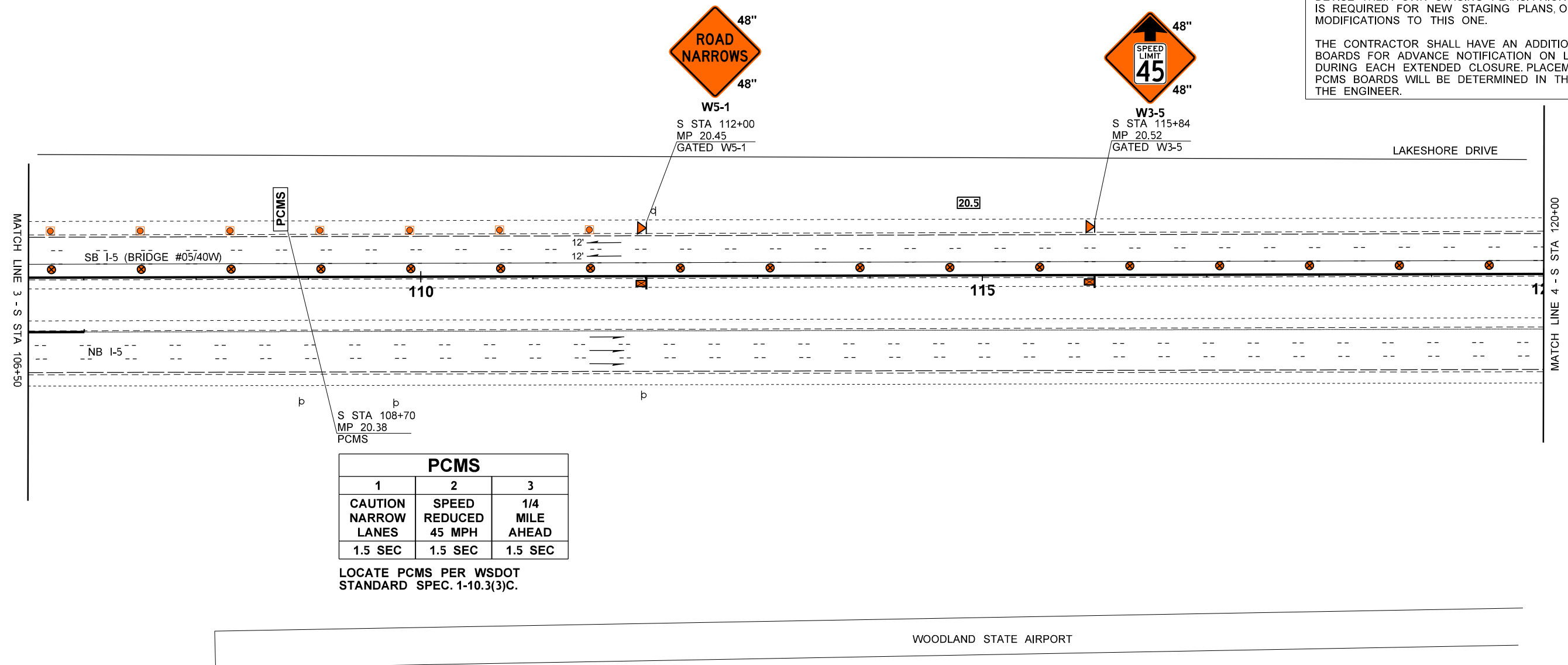
ALL SIGNS ARE 48"x48" BLACK ON ORANGE UNLESS OTHERWISE INDICATED

SIGNS SHOWN WITHIN THESE STAGING PLANS SHALL BE CLASS A AND POST-MOUNTED.

CONFLICTING SIGNS SHALL BE COVERED IN ACCORDANCE WITH STANDARD SPEC. 8-21.3(3) (SIGN COVERING WILL BE PAID UNDER OTHER ITEMS OF WORK).

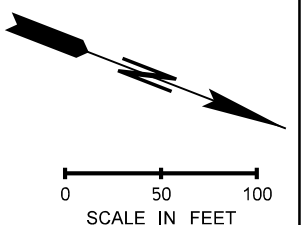
THESE STAGING PLANS ARE FOR GENERAL INFORMATION ONLY AND DO NOT SHOW EVERY STAGE OR CONSTRUCTION ACTIVITY THAT THE CONTRACTOR MAY NEED TO COMPLETE THE PROJECT. WHERE NEEDED, THE CONTRACTOR SHALL DEVISE THEIR OWN STAGING PLANS. PRIOR APPROVAL IS REQUIRED FOR NEW STAGING PLANS, OR FOR MODIFICATIONS TO THIS ONE.

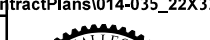

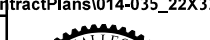
THE CONTRACTOR SHALL HAVE AN ADDITIONAL 3 PCMS BOARDS FOR ADVANCE NOTIFICATION ON LOCAL ROADS DURING EACH EXTENDED CLOSURE. PLACEMENT OF THE PCMS BOARDS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

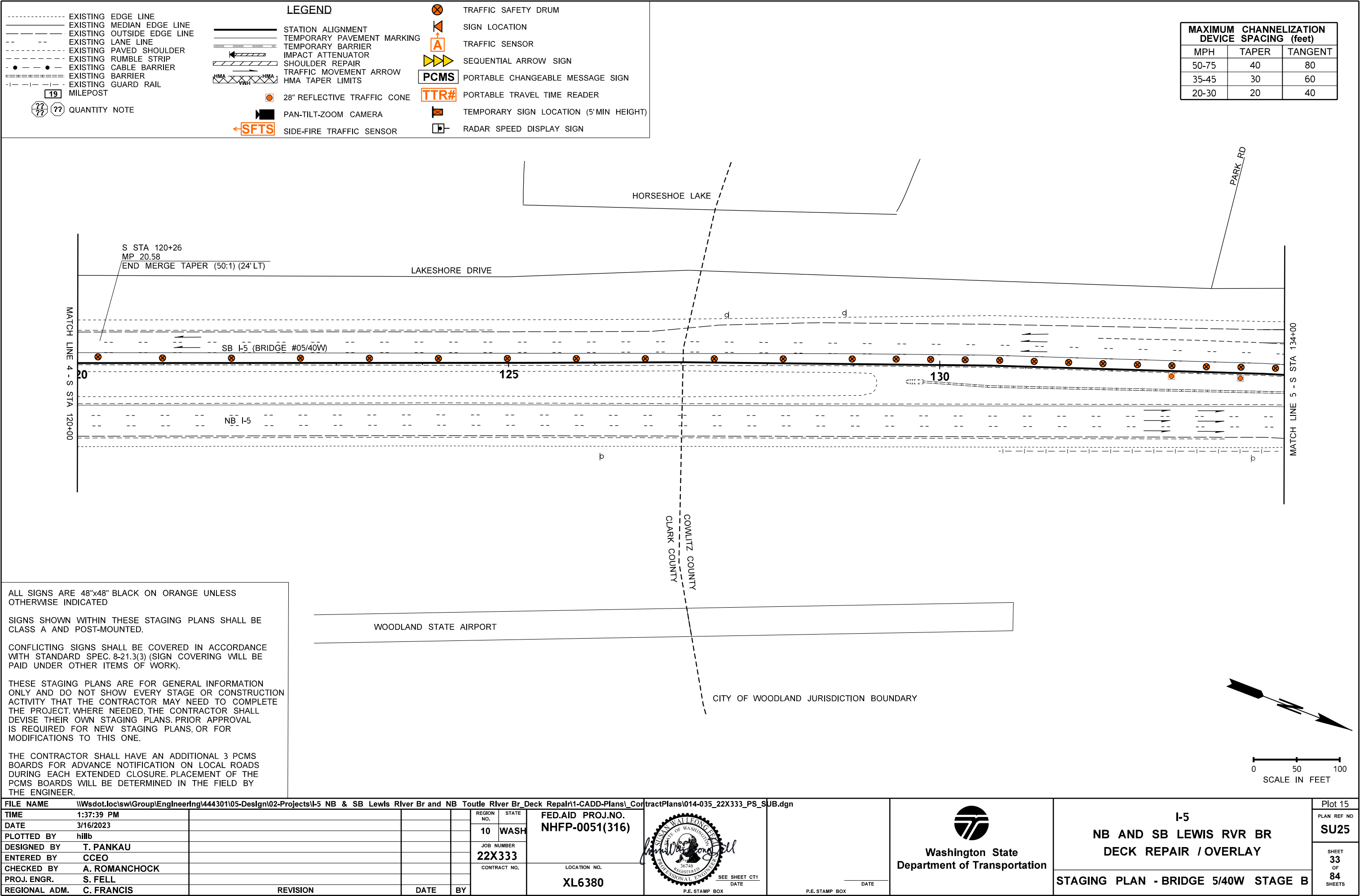


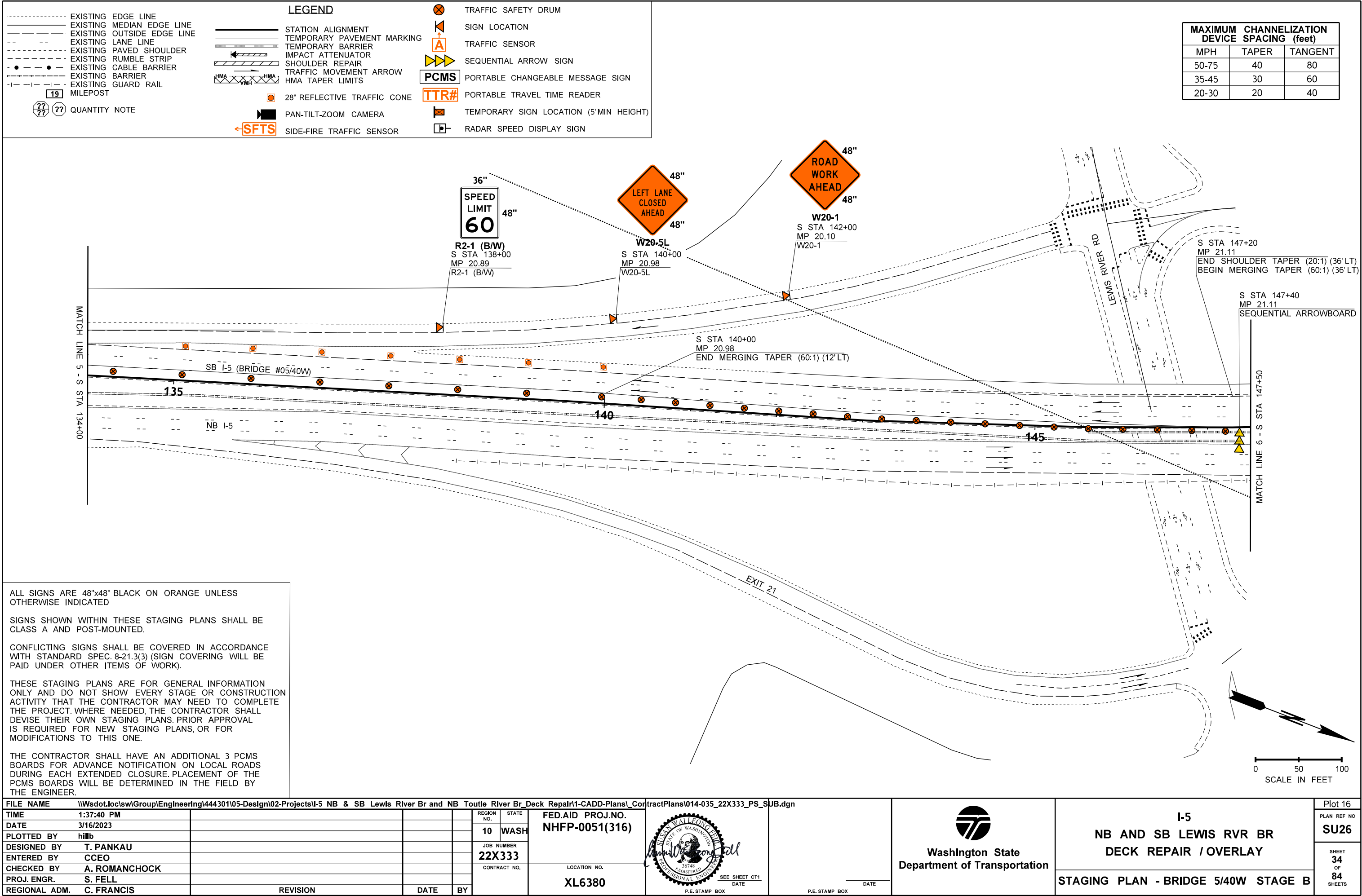
PCMS		
1	2	3
CAUTION NARROW LANES	SPEED REDUCED 45 MPH	1/4 MILE AHEAD
1.5 SEC	1.5 SEC	1.5 SEC

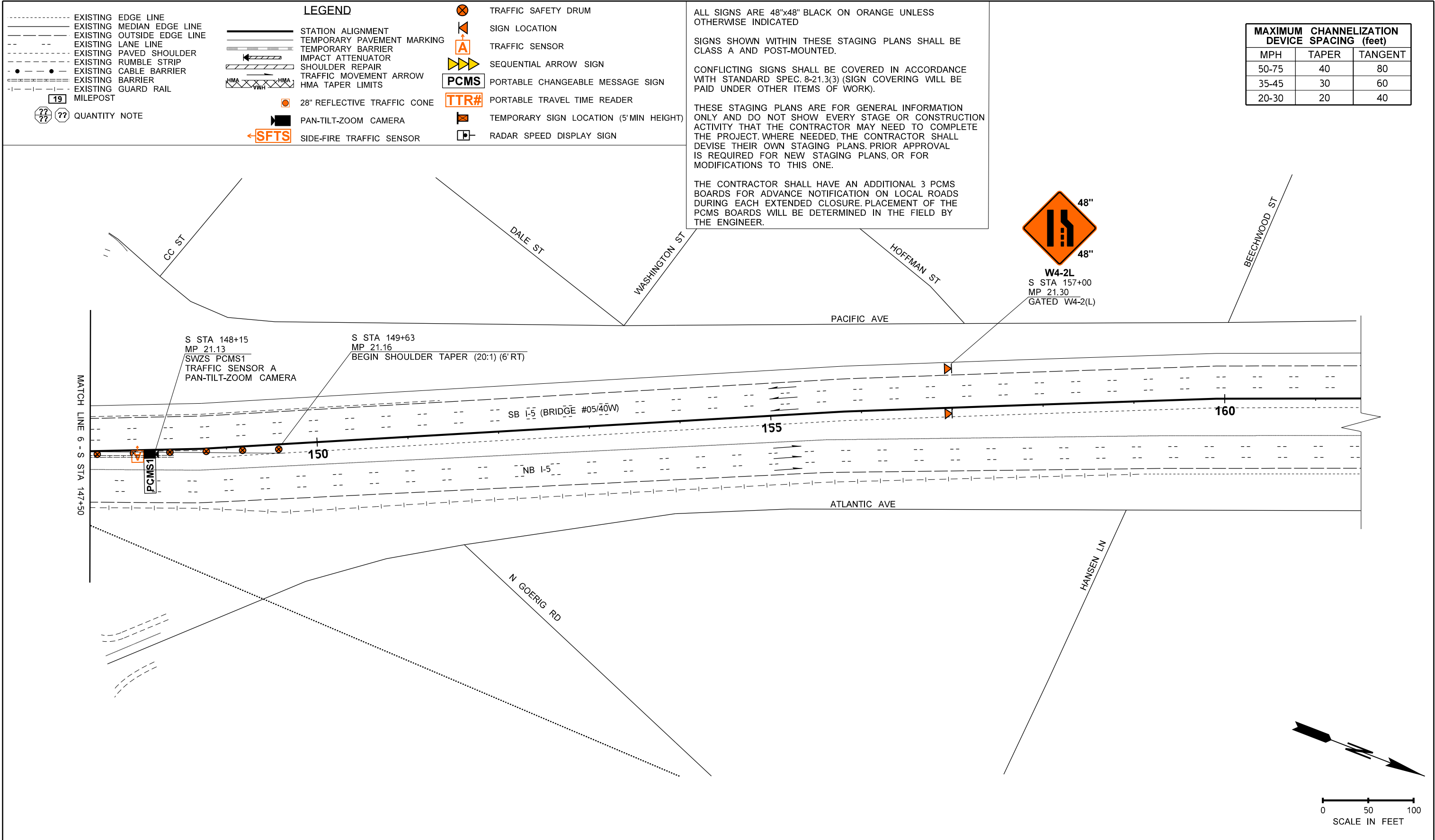
LOCATE PCMS PER WSDOT
STANDARD SPEC. 1-10.3(3)C.



FILE NAME		\\wsdot.local\sw\Group\Engineering\44430105-Design\02-Projects\I-5 NB & SB Lewis River Br and NB Toutle River Br_Deck Repair\1-CADD-Plans\Contract\Plans\014-035_22X333_PS_SUB.dgn										Plot 14													
TIME		1:37:38 PM						REGION NO.		STATE		FED.AID PROJ.NO. NHFP-0051(316)				 Washington State Department of Transportation		I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY		PLAN REF NO					
DATE		3/16/2023						10		WASH										SU24					
PLOTTED BY		hillb						JOB NUMBER				22X333				SEE SHEET CT1 DATE		P.F. STAMP BOX DATE		P.F. STAMP BOX DATE		STAGING PLAN - BRIDGE 5/40W STAGE B		SHEET	
DESIGNED BY		T. PANKAU																						36748 REGISTERED PROFESSIONAL ENGINEER	
ENTERED BY		CCEO						CONTRACT NO.				XL6380		P.F. STAMP BOX DATE		P.F. STAMP BOX DATE		STAGING PLAN - BRIDGE 5/40W STAGE B		OF					
CHECKED BY		A. ROMANCHOCK																		84					
PROJ. ENGR.		S. FELL																		SHEETS					
REGIONAL ADM.		C. FRANCIS																							
				REVISION		DATE		RY																	







----- EXISTING EDGE LINE

----- EXISTING MEDIAN EDGE LINE

----- EXISTING OUTSIDE EDGE LINE

----- EXISTING LANE LINE

----- EXISTING PAVED SHOULDER

----- EXISTING RUMBLE STRIP

----- EXISTING CABLE BARRIER

----- EXISTING BARRIER

----- EXISTING GUARD RAIL

19

MILEPOST

STATION ALIGNMENT

PAVEMENT MARKING

NEW RUMBLE STRIP

TRAFFIC MOVEMENT ARROW

LANE LINE

WHITE EDGE LINE

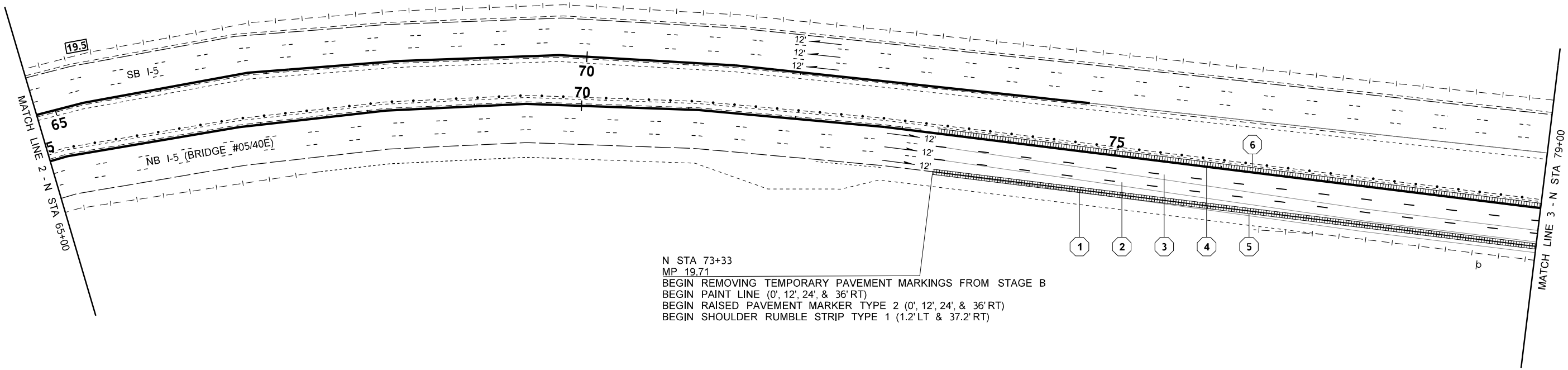
YELLOW EDGE LINE

DRAINAGE INLET MARKING

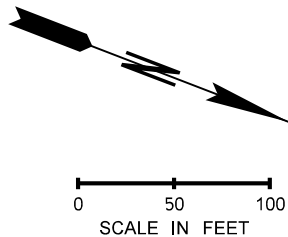
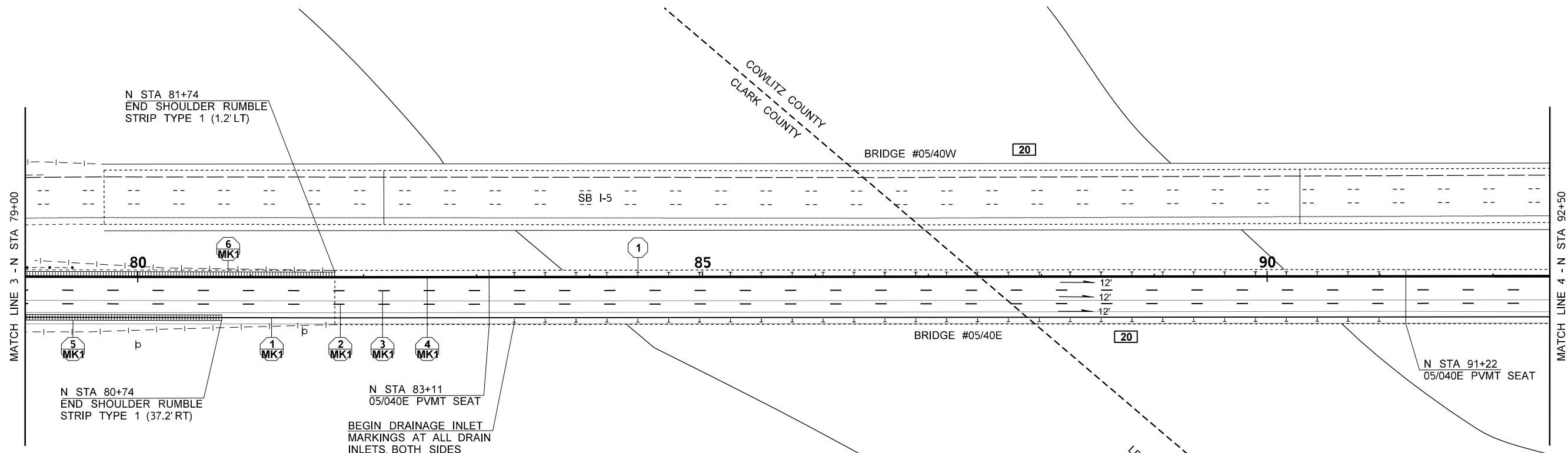
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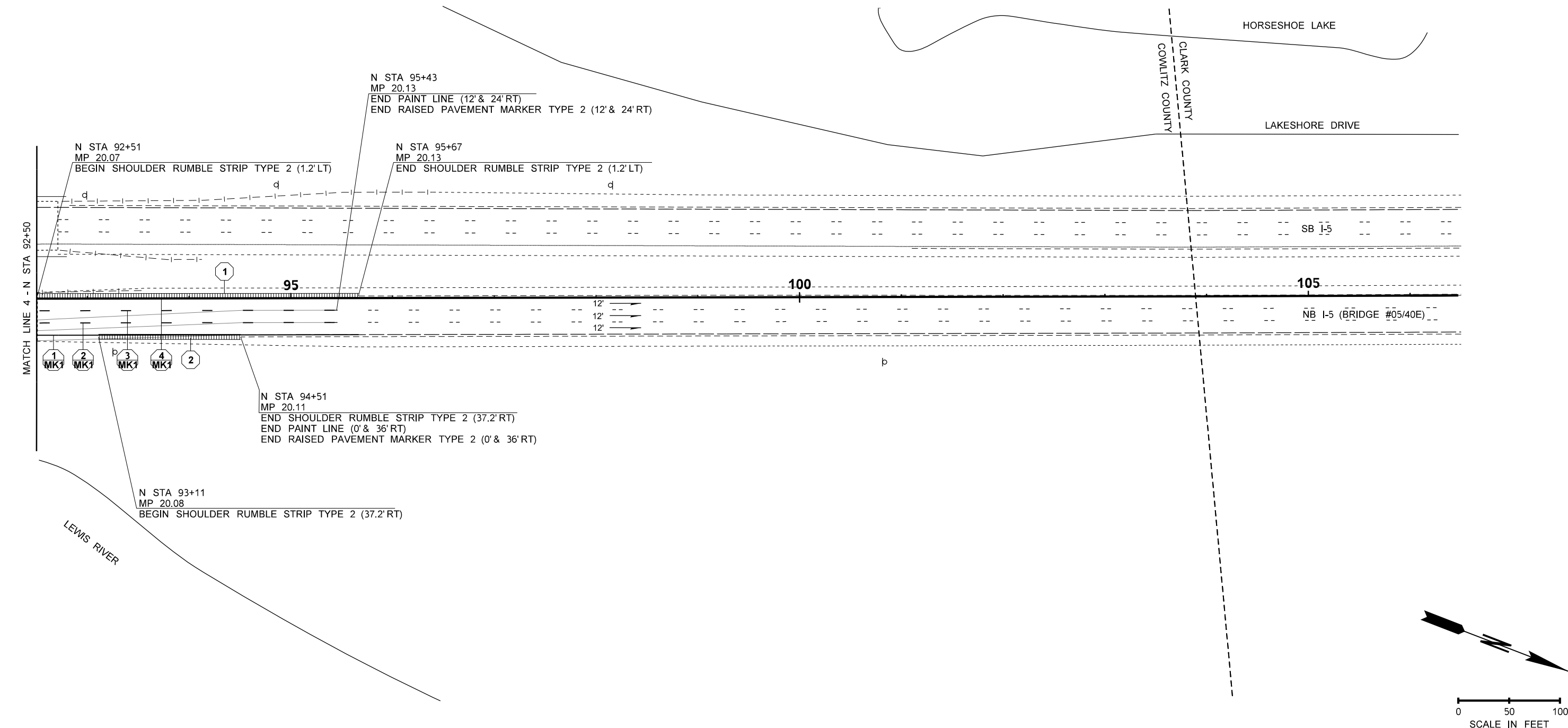
QUANTITY NOTE





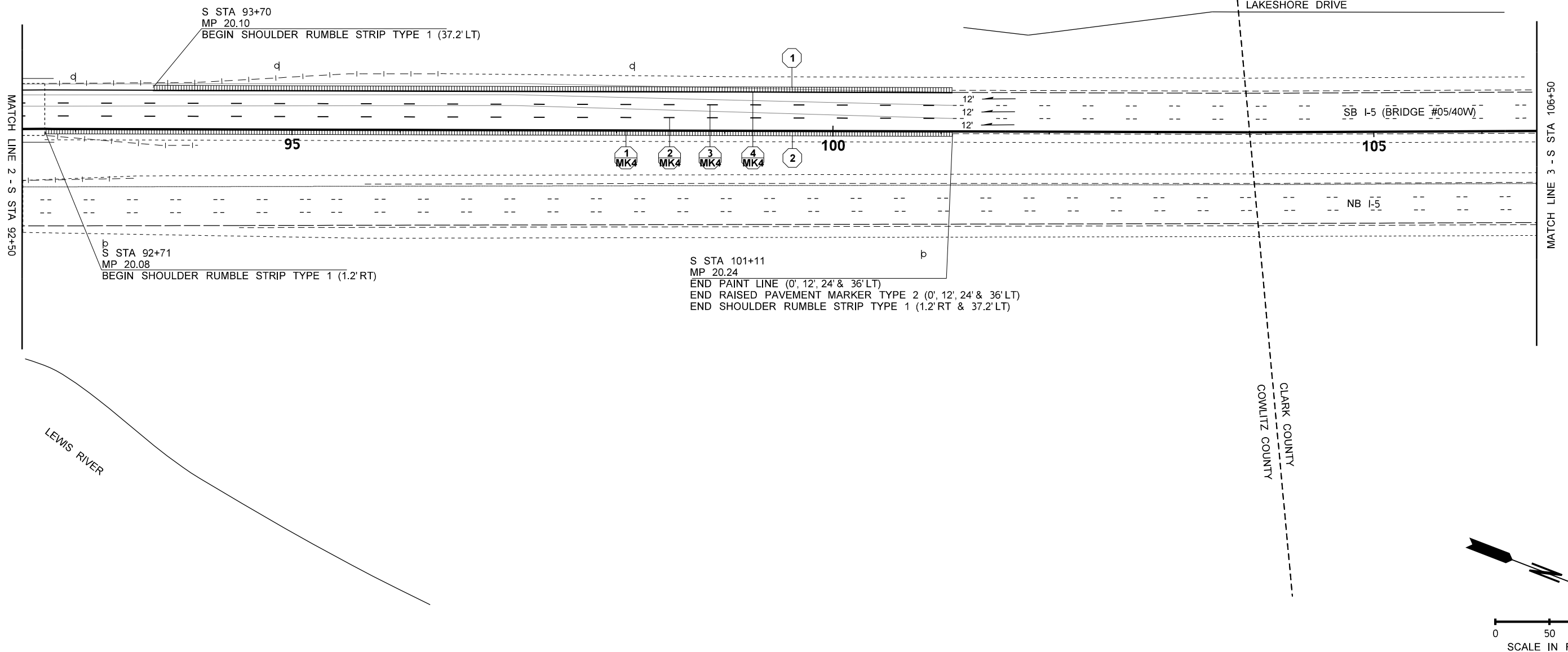
FILE NAME \\Wsdot\locslw\Group\Engineering\444301\05-Design\02-Projects\I-5 NB & SB Lewis River Br and NB Toutle River Br Deck Repair\1-CADD-Plans\ContractPlans\038-043_22X333_PS_MK.dgn										Plot 1
TIME 9:19:12 AM				REGION NO. 10	STATE WASH	FED.AID PROJ.NO. NHFP-0051(316)				PLAN REF NO MK1
DATE 3/14/2023				JOB NUMBER 22X333	CONTRACT NO.					
PLOTTED BY hillb						LOCATION NO. XL6379				SHEET 39 OF 84 SHEETS
DESIGNED BY T. PANKAU										
ENTERED BY CCEO						SEE SHEET CT1 DATE				PAVEMENT MARKING PLAN
CHECKED BY A. ROMANCHOCK										
PROJ. ENGR. S. FELL						DATE				P.E. STAMP BOX
REGIONAL ADM. C. FRANCIS	REVISION	DATE	BY							





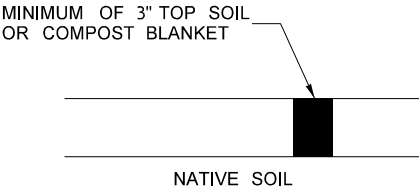
FILE NAME \\Wsdot.loc\sw\Group\Engineering\444301\05-Design\02-Projects\I-5 NB & SB Lewis River Br and NB Toutle River Br Deck Repair\1-CADD-Plans\ContractPlans\038-043_22X333_PS_MK.dgn										Plot 2	
TIME	9:19:08 AM				REGION NO.	STATE	FED.AID PROJ.NO. NHFP-0051(316)				PLAN REF NO MK2
DATE	3/14/2023				10	WASH					
PLOTTED BY	hillb				JOB NUMBER		22X333				SHEET 40 OF 84 SHEETS
DESIGNED BY	T. PANKAU										
ENTERED BY	CCEO				CONTRACT NO.		LOCATION NO.				PAVEMENT MARKING PLAN
CHECKED BY	A. ROMANCHOCK						XL6379				
PROJ. ENGR.	S. FELL										
REGIONAL ADM.	C. FRANCIS				REVISION		DATE	BY			



FILE NAME \\Wsdot\loc\sw\Group\Engineering\444301\05-Design\02-Projects\I-5 NB & SB Lewis River Br and NB Toutle River Br_Deck Repair\1-CADD-Plans\ContractPlans\038-043_22X333_PS_MK.dgn										Plot 3	
TIME	9:19:09 AM				REGION NO.	STATE	FED.AID PROJ.NO.		 Washington State Department of Transportation	I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY	PLAN REF NO
DATE	3/14/2023				10	WASH	NHFP-0051(316)				MK3
PLOTTED BY	hillb				JOB NUMBER						
DESIGNED BY	T. PANKAU				22X333						
ENTERED BY	CCEO				CONTRACT NO.						
CHECKED BY	A. ROMANCHOCK				LOCATION NO.						SHEET 41 OF 84 SHEETS
PROJ. ENGR.	S. FELL				XL6379						PAVEMENT MARKING PLAN
REGIONAL ADM.	C. FRANCIS				REVISION						
					DATE						
					BY						



FILE NAME \\Wsdot.loc\sw\Group\Engineering\444301\05-Design\02-Projects\I-5 NB & SB Lewis River Br and NB Toutle River Br_Deck Repair\1-CADD-Plans\ContractPlans\038-043_22X333_PS_MK.dgn										Plot 13	
TIME	9:19:11 AM				REGION NO.	STATE	FED.AID PROJ.NO.		 Washington State Department of Transportation	I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY	PLAN REF NO
DATE	3/14/2023				10	WASH	NHFP-0051(316)				MK6
PLOTTED BY	hillb				JOB NUMBER						
DESIGNED BY	T. PANKAU				22X333						
ENTERED BY	CCEO				CONTRACT NO.						
CHECKED BY	A. ROMANCHOCK				LOCATION NO.						SHEET
PROJ. ENGR.	S. FELL				XL6380						44
REGIONAL ADM.	C. FRANCIS										OF
		REVISION	DATE	BY							84
											SHEETS



SEEDING, FERTILIZING AND MULCHING

NOTES

1. EXISTING PAVED / GRAVEL DRIVEWAY TO LEWIS RIVER SHALL BE UNOBSTRUCTED AT ALL TIMES AND AVAILABLE FOR STATE AGENCIES TO ACCESS LEWIS RIVER.
2. NO STAGING SHALL BE PERMITTED OUTSIDE OF WSDOT RIGHT OF WAY, OUTSIDE HATCHED AREAS, OR BEHIND HIGH VISIBILITY FENCE UNLESS APPROVED BY THE ENGINEER.
3. NO STAGING SHALL BE PERMITTED UNDER THE SOUTH APPROACH TO EITHER BRIDGE.
4. NO CONSTRUCTION VEHICLE ACCESS SHALL BE PERMITTED FROM THE STAGING YARD DIRECTLY TO I-5.
5. NO TRESPASSING SIGNS SHALL BE INSTALLED AS STAKED BY THE ENGINEER.
6. STAGING YARD SHALL BE RETURNED TO ORIGINAL CONDITIONS AT COMPLETION OF PROJECT. IF CONTRACTOR ELECTS TO INSTALL GRAVEL IN THE STAGING YARD, THE CONTRACTOR SHALL INSTALL WATTLES ON THE DOWNHILL SLOPE AS APPROVED BY THE ENGINEER. INSTALLATION AND REMOVAL OF BOTH ITEMS SHALL BE AT NO COST TO THE CONTRACTING AGENCY, SEE SPECIAL PROVISION "SEEDING, FERTILIZING AND MULCHING".
7. SEE SPECIAL PROVISION ENVIRONMENTAL REGULATIONS.

LEGEND

- APPROVED STAGING YARD
- hvf -

HIGH VISIBILITY FENCE
- x- -x-

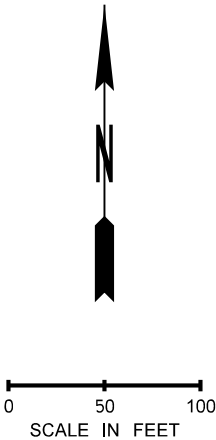
EXISTING FENCE
- - - - -

EXISTING GRAVEL DRIVEWAY
- - - - -

EXISTING PAVED DRIVEWAY
- =====


APPROXIMATE RIGHT OF WAY
- =====


200 FT FROM WATERS OF THE STATE

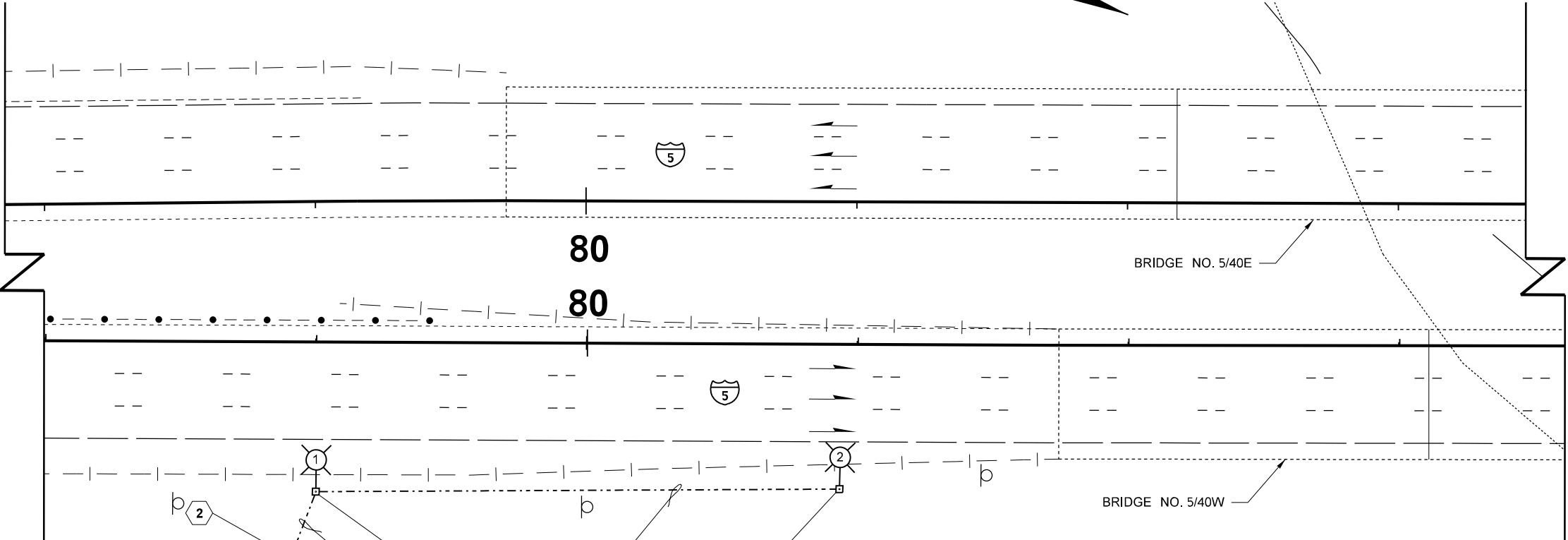


FILE NAME				\\Wsdot.loc\sw\Group\Engineering\444301\05-Design\02-Projects\I-5 NB & SB Lewis River Br and NB Toutle River Br_Deck Repair\1-CADD-Plans\ContractPlans\044_22X333_PS_SUEC.dgn										Plot 1			
TIME				9:50:08 AM										PLAN REF NO			
DATE				3/14/2023										SUEC1			
PLOTTED BY				hillb										SHEET 45 OF 84 SHEETS			
DESIGNED BY				T. PANKAU													
ENTERED BY				CCEO													
CHECKED BY				A. ROMANCHOCK													
PROJ. ENGR.				S. FELL													
REGIONAL ADM.				C. FRANCIS													
				REVISION		DATE		BY									

TEMPORARY ILLUMINATION SYSTEM									
LUMINAIRE NO.	LOCATION		MAST ARM	H1	BASE TYPE	FOUNDATION	DEPTH	WATTAGE	NOTE
	STATION	OFFSET							
1	N 79+00.23	55' RT	16'	40'	TIMBER	-	-	400 EQV.	
2	N 80+93.60	53' RT	16'	40'	TIMBER	-	-	400 EQV.	

	WIRING SCHEDULE:		
WIRE NOTE	CONDUIT	CONDUCTOR SIZE	REMARKS
1	-	#6 TRIPLEX	OVERHEAD

	CONSTRUCTION NOTES:
1	FURNISH AND INSTALL A TEMPORARY 50 FT TIMBER POLE, 16 FT MAST ARM, 400W OR EQV. LUMINAIRE, GUY WIRE, ANCHOR AND ALL NECESSARY COMPONENTS PER STANDARD PLAN J-15.10 FOR A TEMPORARY TIMBER LIGHT STANDARD.
2	FURNISH AND INSTALL A 20 FT TEMPORARY TIMBER POLE. TEMPORARY TIMBER POLE SHALL BE INSTALLED PER STANDARD SPECIFICATION 8-20.3(13)A.
3	FURNISH AND INSTALL A TEMPORARY GENERATOR OR OTHER SOURCE OF POWER FOR A TEMPORARY ILLUMINATION SYSTEM. GENERATOR OR OTHER SOURCE OF TEMPORARY POWER SHALL BE ABLE TO MAINTAIN LIGHTING LEVELS DURING HOURS OF DARKNESS AND/ OR INCLEMENT WEATHER. THE CONTRACTOR SHALL DETERMINE THE SELECTED POWER SOURCE WATTAGE, SIZE, LOCATION, AS WELL AS ANY OTHER ADDITIONAL WIRES, ADAPTERS, BREAKERS, OR ANY OTHER ACCESSORIES NEEDED TO POWER THE TEMPORARY ILLUMINATION SYSTEM.



LEGEND



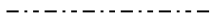
TIMBER LIGHT STANDARD & LUMINAIRE



TEMPORARY ELECTRICAL POWER SOURCE



TEMPORARY TIMBER POLE (OPTIONAL)



WIRE



CONSTRUCTION NOTE





WIRE NOTE


* FOR FEATURES NOT SHOWN ON THIS SHEET REFER TO STAGING PLANS.

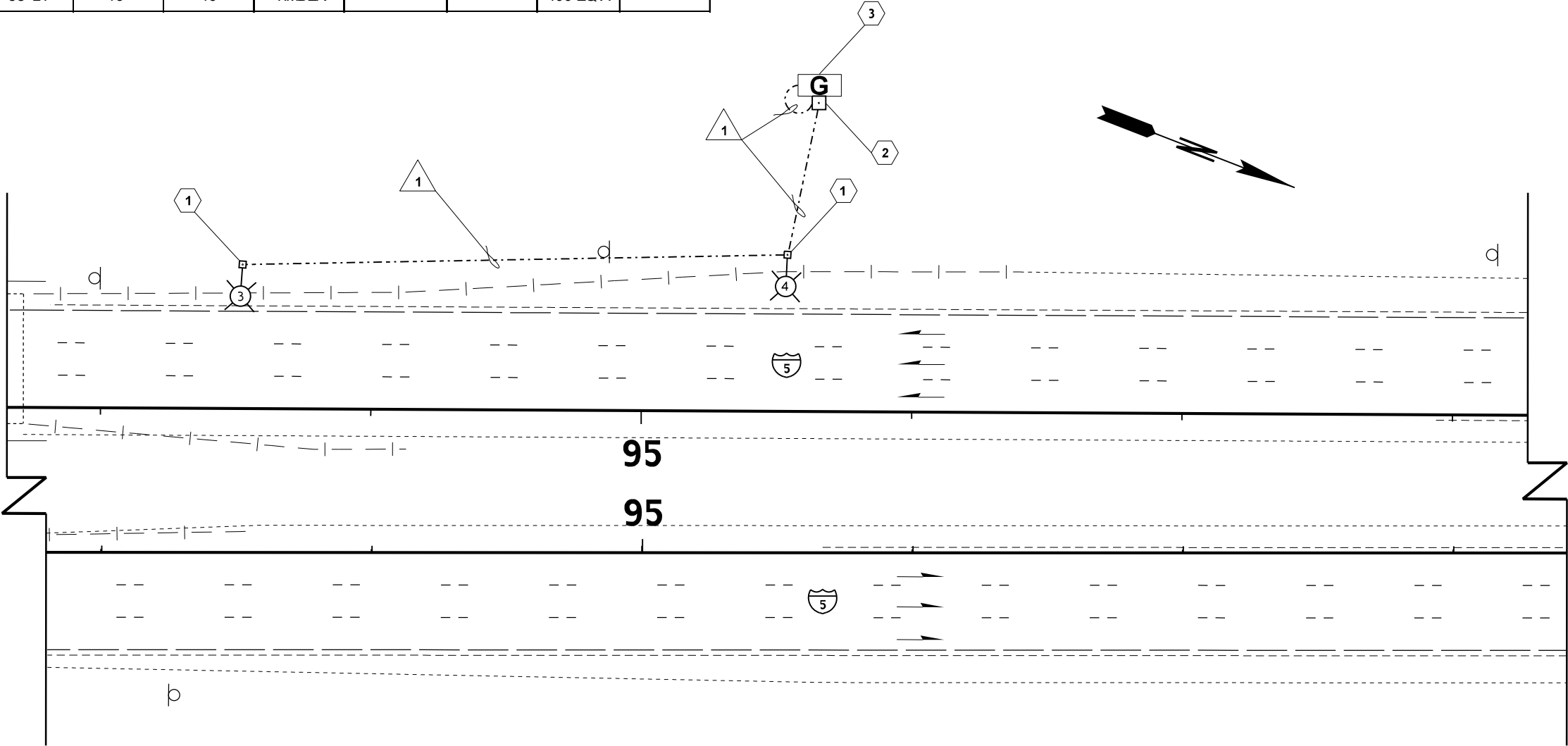
NOTES:

- THE LOCATIONS OF FEATURES SHOWN IN THE PLANS ARE APPROXIMATE AND SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR.
- TEMPORARY ILLUMINATION SHALL BE INSTALLED AND OPERATIONAL PRIOR TO THE CONCRETE BARRIER BEING INSTALLED.
- TEMPORARY ILLUMINATION IS ONLY REQUIRED TO BE ACTIVE AND MAINTAINED WHEN TEMPORARY CONCRETE BARRIER IS IN PLACE.
- CONTRACTOR MAY ELECT TO USE TEMPORARY PORTABLE LIGHT PLANTS IN LIEU OF TEMPORARY TIMBER POLES. IF THE CONTRACTOR ELECTS TO USE TEMPORARY PORTABLE LIGHT PLANTS, THE CONTRACTOR SHALL SUBMIT A TEMPORARY LIGHTING PLAN TO THE TRAFFIC ENGINEER FOR APPROVAL PRIOR TO USE. APPROVAL OF THE TEMPORARY LIGHTING PLAN MAY TAKE UP TO 5 WORKING DAYS PER SUBMITTAL/REVISION. TEMPORARY LIGHTING AND APPROVAL SHALL BE INCIDENTAL TO THE BID ITEM "TEMPORARY ILLUMINATION SYSTEM". CONTRACTOR SHALL REFER TO WSDOT 2022 DESIGN MANUAL CHAPTER 1040 FOR LIGHTING REQUIREMENTS. TEMPORARY LIGHTING PLAN SHALL INCLUDE, BUT NOT BE LIMITED TO THE LOCATION(S) OF THE TEMPORARY LIGHT PLANTS, LIGHTING LEVEL CALCULATIONS AND LAYOUT. REFER TO WSDOT DESIGN MANUAL SECTION 1040.06, EXHIBIT 1040-39 AND 1040-45.
- DURING HOURS OF DARKNESS AND/OR INCLEMENT WEATHER THE CONTRACTOR SHALL MAINTAIN LIGHTING LEVELS THROUGHOUT THE PROJECT. HOURS OF DARKNESS ARE DEFINED AS 1/2 HOUR PRIOR TO SUNSET AND 1/2 HOUR AFTER SUN RISE.
- ALL ITEMS ON THIS SHEET, INCLUDING THE REMOVAL OF THE TEMPORARY ILLUMINATION SYSTEM, PUD REQUIREMENTS, BILLING OR ANY OTHER ASSOCIATED MATERIALS NEEDED TO PROVIDE TEMPORARY ILLUMINATION SHALL BE INCIDENTAL TO THE BID ITEM "TEMPORARY ILLUMINATION SYSTEM".

FILE NAME										G:\Traffic\TRAFFIC DESIGN\01-ACTIVE DESIGN FILES\XL6378_I-5 Bridge Overlay\Base\XL6378_PS_TIL.dgn										Plot 1																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
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LUMINAIRE NO.	LOCATION		MAST ARM	H1	BASE TYPE	FOUNDATION	DEPTH	WATTAGE	NOTE
	STATION	OFFSET							
3	S 93+52.34	53' LT	16'	40'	TIMBER	-	-	400 EQV.	
4	S 95+53.80	58' LT	16'	40'	TIMBER	-	-	400 EQV.	

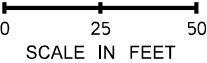
	WIRING SCHEDULE:		
WIRE NOTE	CONDUIT	CONDUCTOR SIZE	REMARKS
1	-	#6 TRIPLEX	OVERHEAD



NOTES:

- THE LOCATIONS OF FEATURES SHOWN IN THE PLANS ARE APPROXIMATE AND SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR.
- TEMPORARY ILLUMINATION SHALL BE INSTALLED AND OPERATIONAL PRIOR TO THE TEMPORARY CONCRETE BARRIER BEING INSTALLED.
- TEMPORARY ILLUMINATION IS ONLY REQUIRED TO BE ACTIVE AND MAINTAINED WHEN TEMPORARY CONCRETE BARRIER IS IN PLACE.
- CONTRACTOR MAY ELECT TO USE TEMPORARY PORTABLE LIGHT PLANTS IN LIEU OF TEMPORARY TIMBER POLES. IF THE CONTRACTOR ELECTS TO USE TEMPORARY PORTABLE LIGHT PLANTS, THE CONTRACTOR SHALL SUBMIT A TEMPORARY LIGHTING PLAN TO THE TRAFFIC ENGINEER FOR APPROVAL PRIOR TO USE. APPROVAL OF THE TEMPORARY LIGHTING PLAN MAY TAKE UP TO 5 WORKING DAYS PER SUBMITTAL/REVISION. TEMPORARY LIGHTING AND APPROVAL SHALL BE INCIDENTAL TO THE BID ITEM "TEMPORARY ILLUMINATION SYSTEM". CONTRACTOR SHALL REFER TO WSDOT 2022 DESIGN MANUAL CHAPTER 1040 FOR LIGHTING REQUIREMENTS. TEMPORARY LIGHTING PLAN SHALL INCLUDE, BUT NOT BE LIMITED TO, THE LOCATION(S) OF THE TEMPORARY LIGHT PLANTS, LIGHTING LEVEL CALCULATIONS AND LAYOUT. REFER TO WSDOT DESIGN MANUAL SECTION 1040.06, EXHIBIT 1040-39 AND 1040-45.
- DURING HOURS OF DARKNESS AND/OR INCLEMENT WEATHER THE CONTRACTOR SHALL MAINTAIN LIGHTING LEVELS THROUGHOUT THE PROJECT. HOURS OF DARKNESS ARE DEFINED AS 1/2 HOUR PRIOR TO SUNSET AND 1/2 HOUR AFTER SUN RISE.
- ALL ITEMS ON THIS SHEET, INCLUDING THE REMOVAL OF THE TEMPORARY ILLUMINATION SYSTEM, PUD REQUIREMENTS, BILLING OR ANY OTHER ASSOCIATED MATERIALS NEEDED TO PROVIDE TEMPORARY ILLUMINATION SHALL BE INCIDENTAL TO THE BID ITEM "TEMPORARY ILLUMINATION SYSTEM".
- REFER TO PLAN SHEET TIL1 FOR LEGEND.

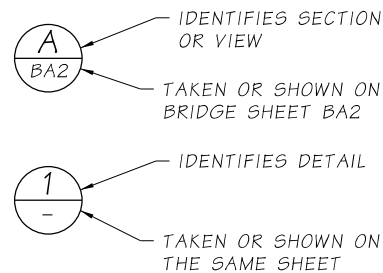
	CONSTRUCTION NOTES:
1	FURNISH AND INSTALL A TEMPORARY 50 FT TIMBER POLE, 16 FT MAST ARM, 400W OR EQV. LUMINAIRE, GUY WIRE, ANCHOR AND ALL NECESSARY COMPONENTS PER STANDARD PLAN J-15.10 FOR A TEMPORARY TIMBER LIGHT STANDARD.
2	FURNISH AND INSTALL A 20 FT TEMPORARY TIMBER POLE. TEMPORARY TIMBER POLE SHALL BE INSTALLED PER STANDARD SPECIFICATION 8-20.3(13)A.
3	FURNISH AND INSTALL A TEMPORARY GENERATOR OR OTHER SOURCE OF POWER FOR A TEMPORARY ILLUMINATION SYSTEM. GENERATOR OR OTHER SOURCE OF TEMPORARY POWER SHALL BE ABLE TO MAINTAIN LIGHTING LEVELS DURING HOURS OF DARKNESS AND/ OR INCLEMENT WEATHER. THE CONTRACTOR SHALL DETERMINE THE SELECTED POWER SOURCE WATTAGE, SIZE, LOCATION, AS WELL AS ANY OTHER ADDITIONAL WIRES, ADAPTERS, BREAKERS, OR ANY OTHER ACCESSORIES NEEDED TO POWER THE TEMPORARY ILLUMINATION SYSTEM.



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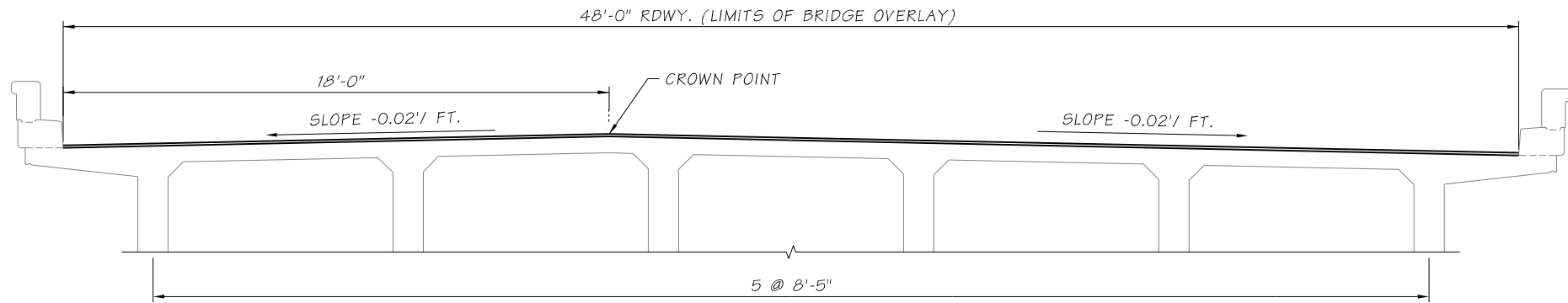


1. ALL MATERIAL AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE WASHINGTON STATE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD, BRIDGE, AND MUNICIPAL CONSTRUCTION, 2023.
2. THE REHABILITATION DESIGN HAS BEEN IN ACCORDANCE WITH WSDOT BRIDGE DESIGN MANUAL (2022) AND AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS - 9TH EDITION, 2020.
3. EXISTING FEATURES AND DIMENSIONS ARE BASED ON INSPECTION REPORTS AND AS-BUILT PLANS. ALL RELEVANT DIMENSIONS AND EXISTING CONDITIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO ORDERING MATERIALS OR STARTING WORK.
4. SEE SHEET BA3 FOR CONSTRUCTION LOAD LIMITS.

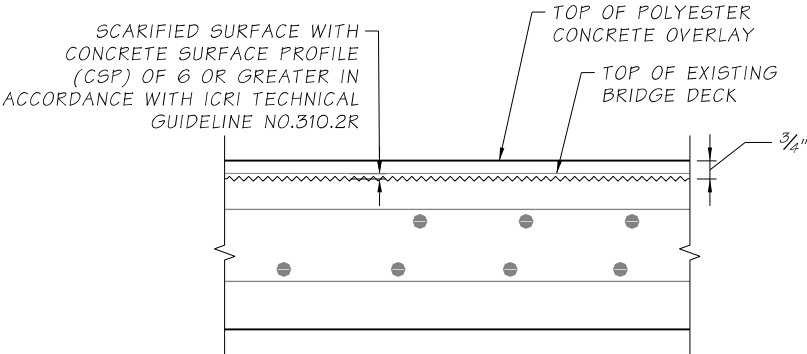


POLYESTER CONCRETE OVERLAY,
JOINT SEALING, AND EXPANSION
JOINT MODIFICATIONS

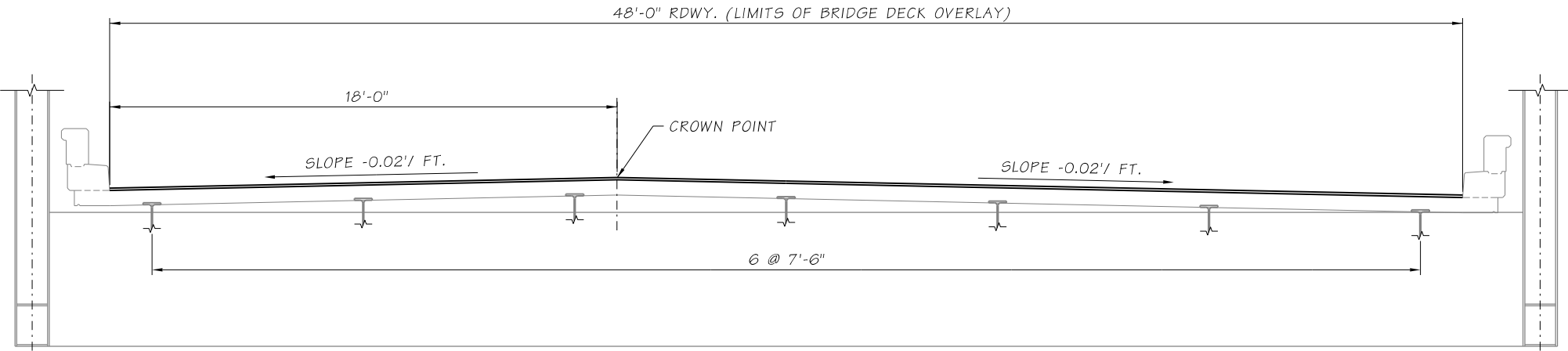
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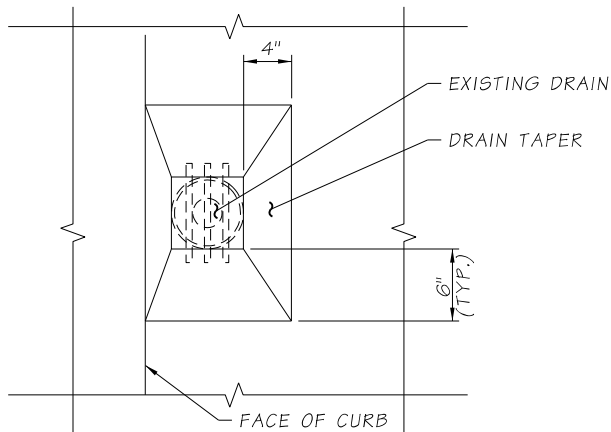
TYPICAL BRIDGE SECTION AT APPROACH SPANS
LOOKING NORTH



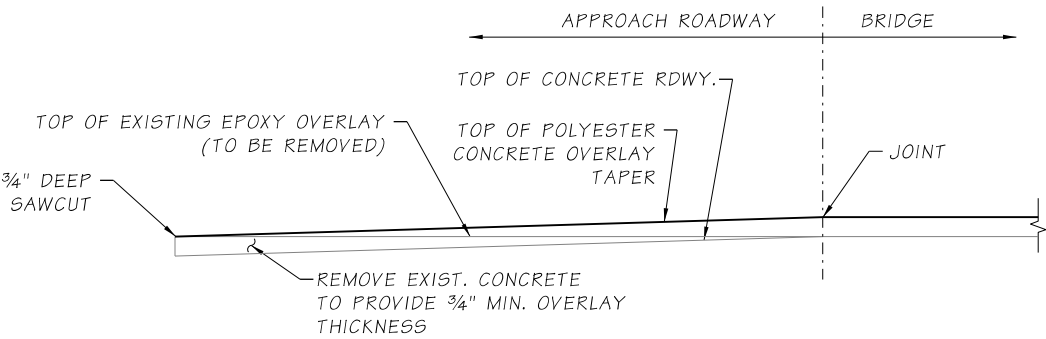
POLYESTER CONCRETE OVERLAY
TYPICAL DETAIL



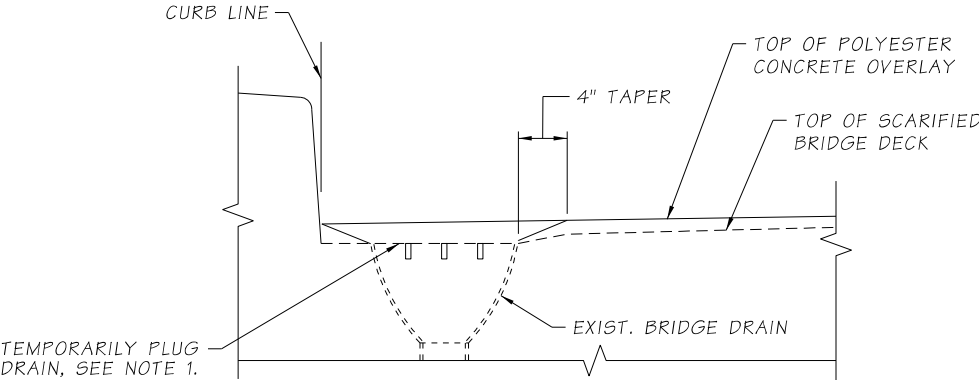
TYPICAL BRIDGE SECTION AT TRUSS SPANS
LOOKING NORTH



PLAN
OVERLAY TAPER AT DRAIN



SOUTH APPROACH TAPER
A
BA1



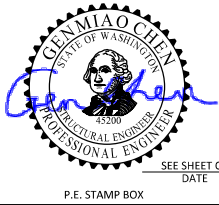
TYPICAL SECTION
OVERLAY TAPER AT DRAIN

- NOTES:
1. TEMPORARILY PLUG EXISTING DRAIN HOLE BEFORE WORK, REMOVE PLUG AFTER OVERLAY. SUBMIT PLUG DETAIL TO THE ENGINEER AS A TYPE 2 WORKING DRAWING.
 2. LONGITUDINAL JOINTS IN THE OVERLAY MAY ONLY BE LOCATED AT THE CROWN POINT, AT THE LANE LINES, OR AT THE CENTER OF LANES, UNLESS APPROVED OTHERWISE BY THE ENGINEER.

FILE NO. SHEET 49 OF 84 SHEETS

FILE NAME X:\211129 WSDOT SW Bridge Deck & Exp\CAD\Bridge 5-40E\IDGN\02 BR5-40E BA2 Typical Section.dgn			
Bridge Desgn Engr	A.LELAND		
Supervisor			
Designed By	G.CHEN		
Checked By	J.LI		
Detailed By	T.LOZADA		
Bridge Projects Engr	M.ROSA		
Prelim. Plan By			
Architect/Specialist			
REVISION		DATE	BY
REGION NO.	10	STATE	WASH
JOB NUMBER	22X333	FED.AID PROJ.NO. NHFP-0051(316)	
CONTRACT NO.		LOCATION NO. XL6379, XL6380	

BRIDGE AND STRUCTURES OFFICE



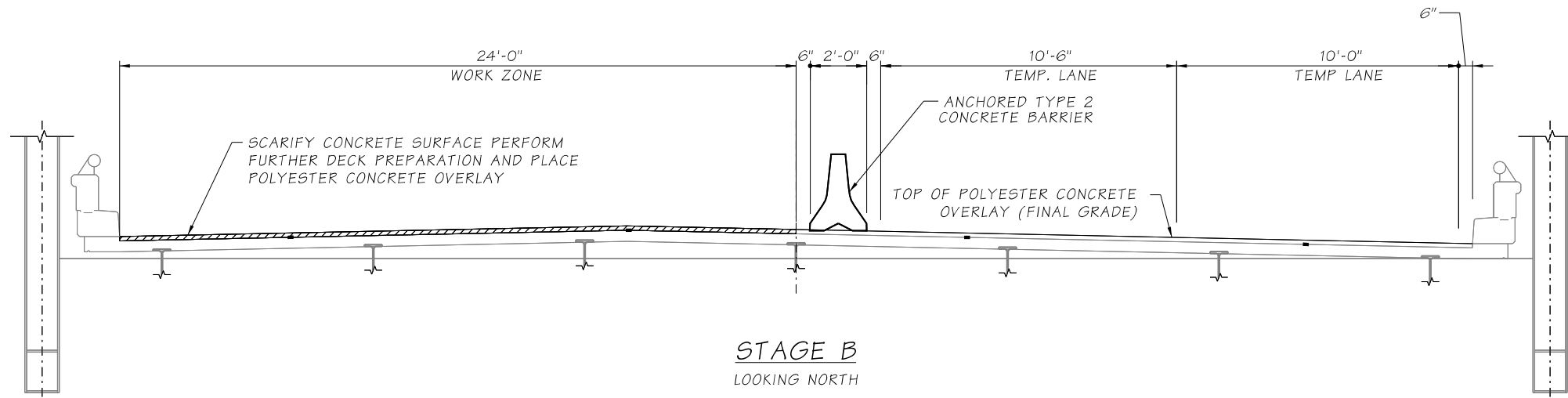
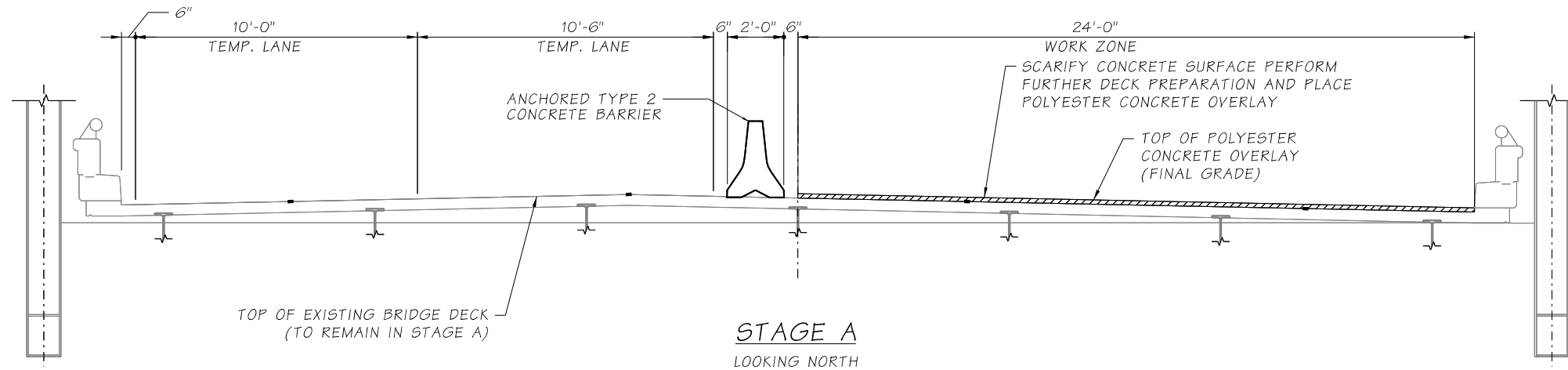
I-5
NB AND SB LEWIS RVR BR
DECK REPAIR / OVERLAY

BRIDGE NO. 5/40E

TYPICAL SECTIONS AND
POLYESTER OVERLAY DETAILS




BRIDGE SHEET NO. BA2
SHEET 49 OF 84 SHEETS

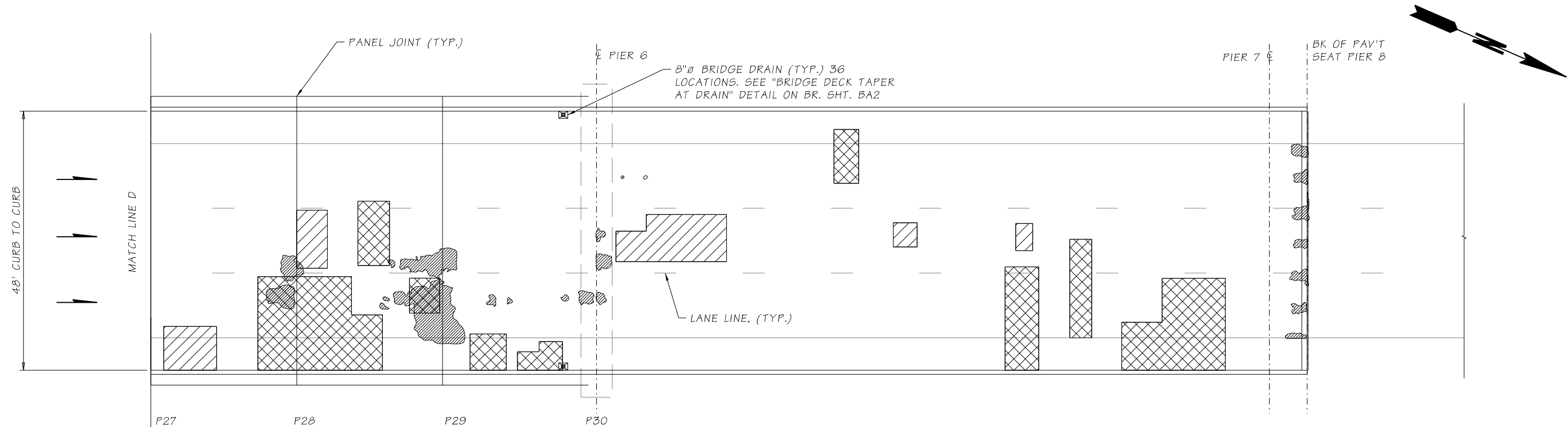
FILE NO. SHEET



CONSTRUCTION LOAD LIMITS:

1. TRAFFIC SHALL BE CONFIGURED AS SHOWN ON THIS PLAN SHEET.
2. ONLY ONE LINE OF TEMPORARY CONCRETE BARRIER IS ALLOWED ON THE BRIDGE. THE WEIGHT OF THE TEMPORARY CONCRETE BARRIER SHALL NOT EXCEED 410 PLF.
3. THE MAXIMUM TOTAL CONSTRUCTION LOAD ON THE BRIDGE, INCLUDING EQUIPMENT AND MATERIALS (EXCEPT FOR THE TEMPORARY CONCRETE BARRIER), SHALL NOT EXCEED 81,000 LBS.
4. THE MAXIMUM TOTAL CONSTRUCTION LOAD TRIBUTARY TO ANY OF THE 62 LOWER PANEL POINTS, INCLUDING EQUIPMENT AND MATERIALS (EXCEPT FOR THE TEMPORARY CONCRETE BARRIER), SHALL NOT EXCEED 40,500 LBS.
5. WORKING DRAWINGS AND CALCULATIONS (TYPE 2E) SHALL BE SUBMITTED IN ACCORDANCE WITH STANDARD SPECIFICATION SECTION 1-05.3. WORKING DRAWINGS AND CALCULATIONS SHALL SHOW THAT ALL OF THE ABOVE CONSTRAINTS HAVE BEEN MET FOR ALL CONSTRUCTION LOADS. CONSTRUCTION LOADS SHALL NOT BE PLACED ON THE BRIDGE UNTIL THE ASSOCIATED WORKING DRAWINGS AND CALCULATIONS HAVE BEEN REVIEWED AND ACCEPTED BY WSDOT.
6. REMOVED MATERIAL, DEBRIS, AND SPENT ABRASIVE BLAST MEDIA SHALL BE COLLECTED AND REMOVED FROM THE BRIDGE DAILY, AT A MINIMUM, TO KEEP UNNECESSARY WEIGHT OFF THE BRIDGE.

FILE NAME X:\211129 WSDOT SW Bridge Deck & Exp\CAD\Bridge 5-40E\IDGN\03 BR5-40E BA3 Construction Sequence.dgn										BRIDGE AND STRUCTURES OFFICE		<div> SEE SHEET CT3 DATE P.E. STAMP BOX</div>		<div><div>Washington State Department of Transportation</div></div> <div><div>RHC ENGINEERING</div></div>		I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY BRIDGE NO. 5/40E		BRIDGE SHEET NO. BA3	
SHEET 50 OF 84 SHEETS																			
		CONSTRUCTION STAGING AND LOAD LIMITS																	
Bridge Desgn Engr A.LELAND						REGION NO. STATE		FED.AID PROJ.NO. NHFP-0051(316)											
Supervisor						10 WASH													
Designed By G.CHEN						JOB NUMBER 22X333													
Checked By J.LI						CONTRACT NO.		LOCATION NO. XL6379, XL6380											
Detailed By T.LOZADA																			
Bridge Projects Engr M.ROSA																			
Prelim. Plan By																			
Architect/Specialist		REVISION		DATE		BY													



PLAN
FURTHER DECK PREPARATION

NOTE:

SEE SHEET BA4 FOR REPAIR NOTES.

LEGENDS

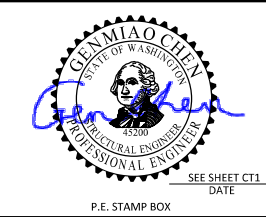
- TYPE 2 DECK REPAIR, SEE DETAIL ON SHEET BA7
- TYPE 1 DECK REPAIR, SEE DETAIL ON SHEET BA7
- PREVIOUS BRIDGE DECK REPAIR TO BE REMOVED AND REPLACED.

SR FILE NO. SHEET

FILE NAME X:\211129 WSDOT SW Bridge Deck & Exp\CAD\Bridge 5-40E\IDGN\04-06 BR5-40E Deck Preparation Plan Details.dgn			
Bridge Desgn Engr	A.LELAND		
Supervisor			
Designed By	G. CHEN		
Checked By	J.LI		
Detailed By	Y.WANG		
Bridge Projects Engr	M.ROSA		
Prelim. Plan By			
Architect/Specialist			
REVISION		DATE	BY

REGION NO.	STATE	FED.AID PROJ.NO.
10	WASH	NHFP-0051(316)
JOB NUMBER		
22X333		
CONTRACT NO.		LOCATION NO.
		XL6379, XL6380

BRIDGE
AND
STRUCTURES
OFFICE



Washington State
Department of Transportation

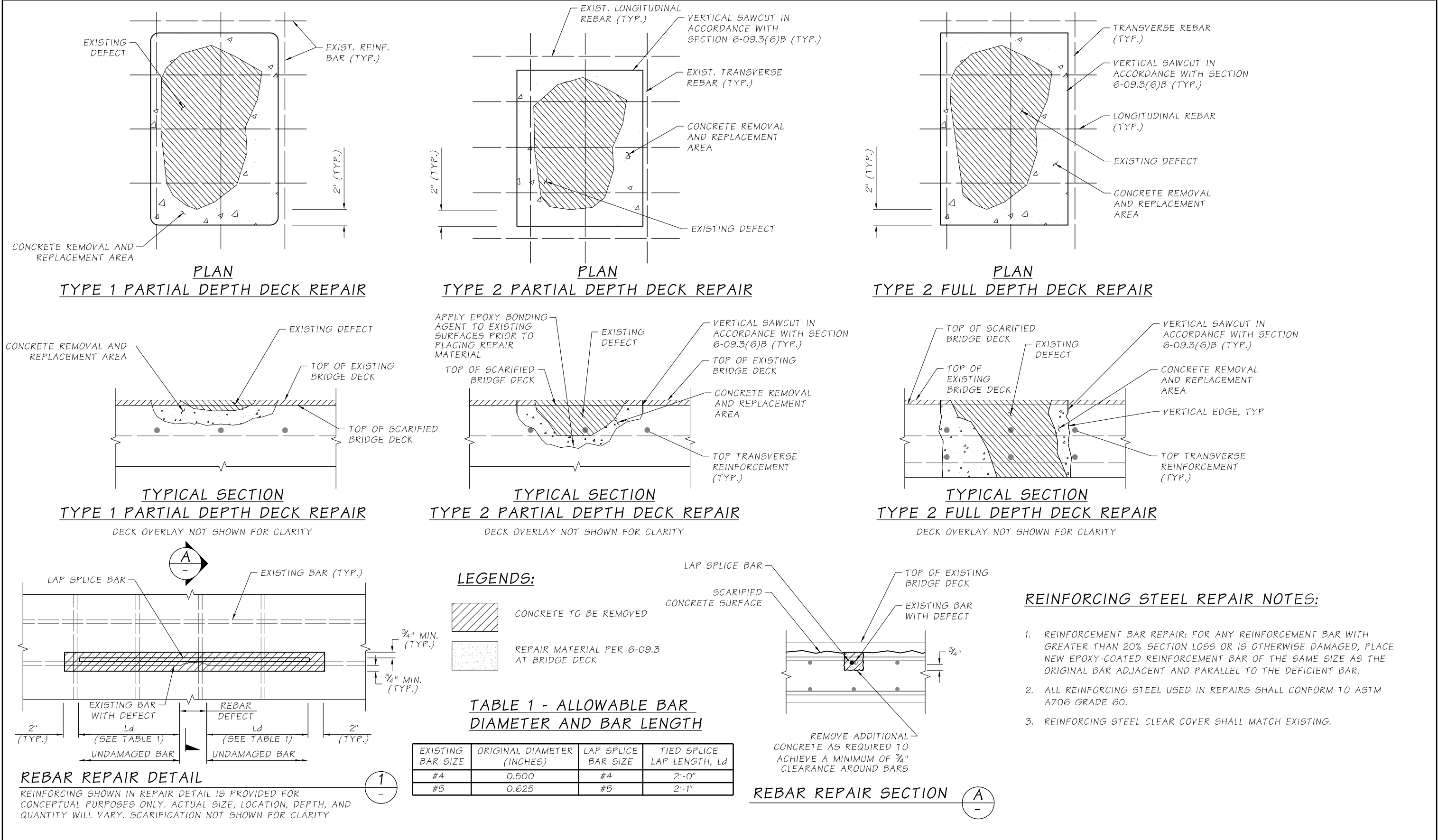


RHC ENGINEERING

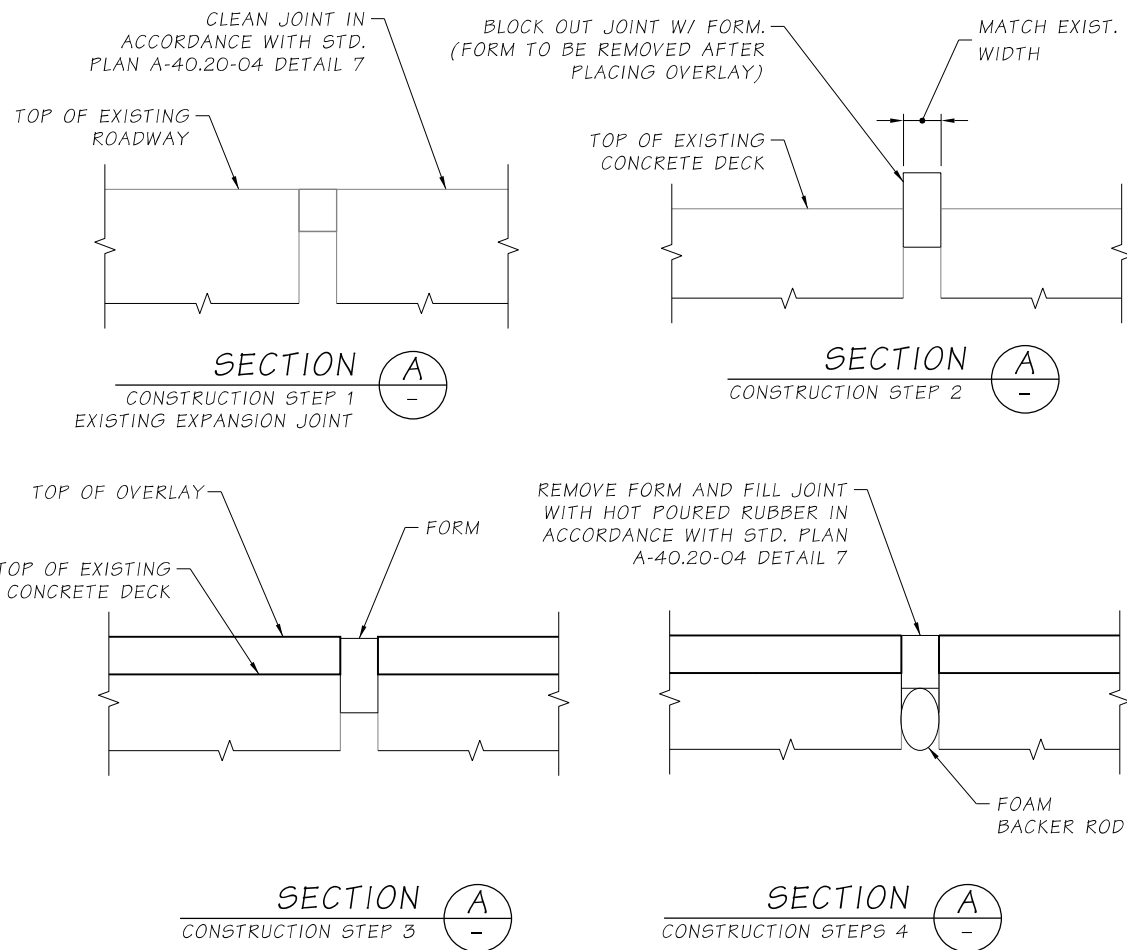
I-5
NB AND SB LEWIS RVR BR
DECK REPAIR / OVERLAY
BRIDGE NO. 5/40E

FURTHER DECK PREPARATION PLAN 3 OF 3

BRIDGE
SHEET NO.
BA6
SHEET
53
OF
84
SHEETS

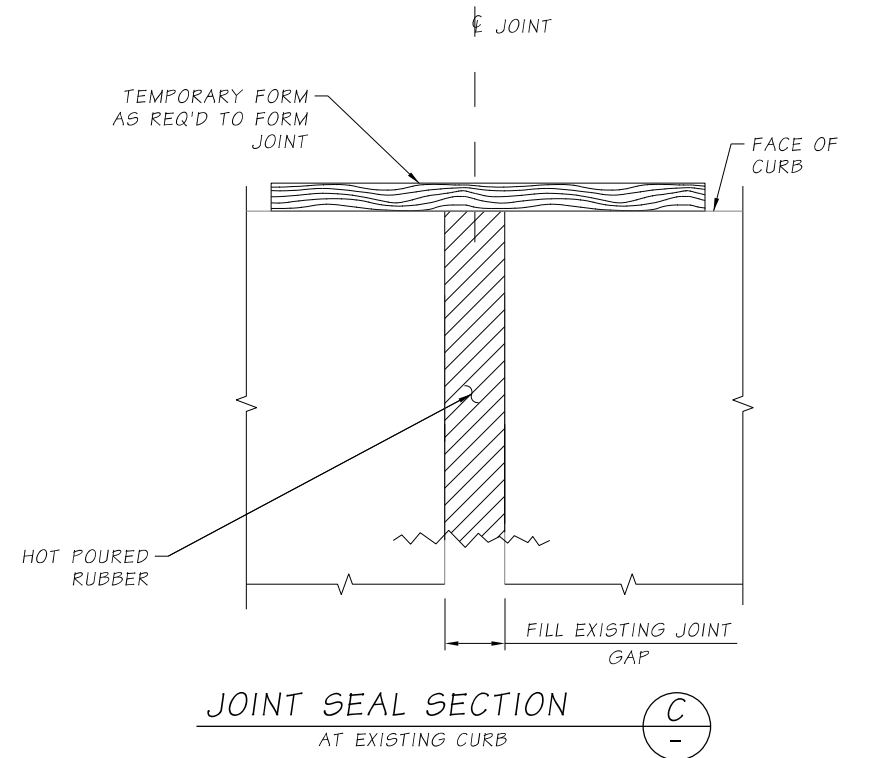
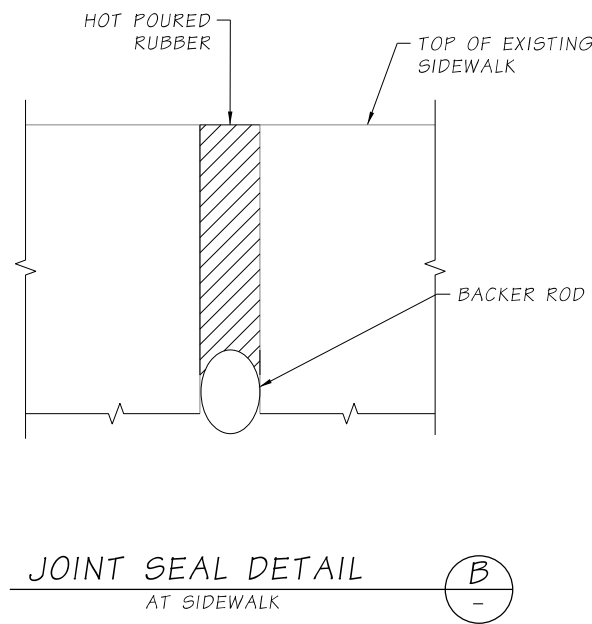
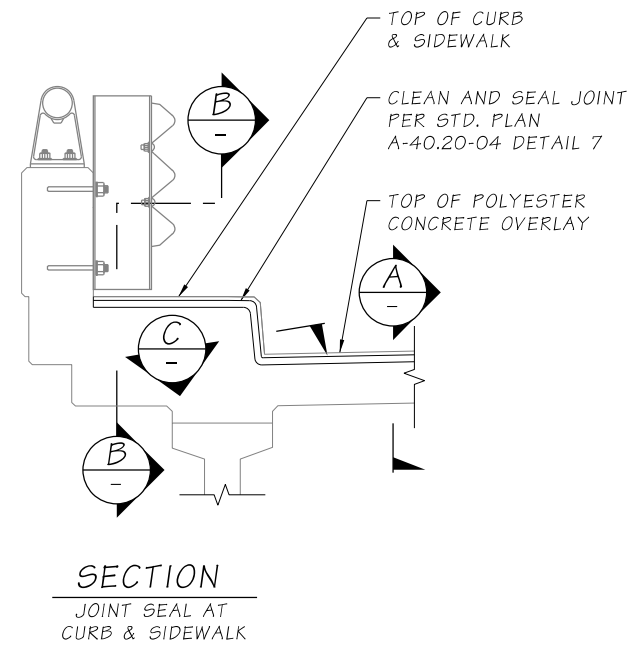


FILE NO. SHEET



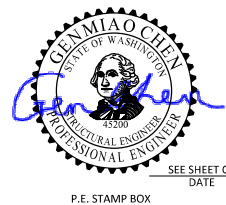
SUGGESTED CONSTRUCTION SEQUENCE:

- STEP 1. CLEAN JOINT
- STEP 2. PLACE FORM IN EXISTING JOINT OPENING.
- STEP 3. PLACE POLYESTER CONCRETE OVERLAY TO FINAL RDWY. ELEVATION.
- STEP 4. REMOVE FORM FROM JOINT OPENING AND FILL WITH HOT POURED RUBBER.



FILE NAME X:\211129 WSDOT SW Bridge Deck & Exp\CAD\Bridge 5-40E\IDGN\08 BR5-40E BA8 Seal Joint Modification Details.dgn									
Bridge Desgn Engr	A.LELAND					REGION NO.	STATE	FED.AID PROJ.NO.	
Supervisor						10	WASH	NHFP-0051(316)	
Designed By	G.CHEN					JOB NUMBER			
Checked By	J.LI					22X333			
Detailed By	T.LOZADA					CONTRACT NO.		LOCATION NO.	
Bridge Projects Engr	M.ROSA							XL6379, XL6380	
Prelim. Plan By									
Architect/Specialist									
REVISION					DATE	BY			

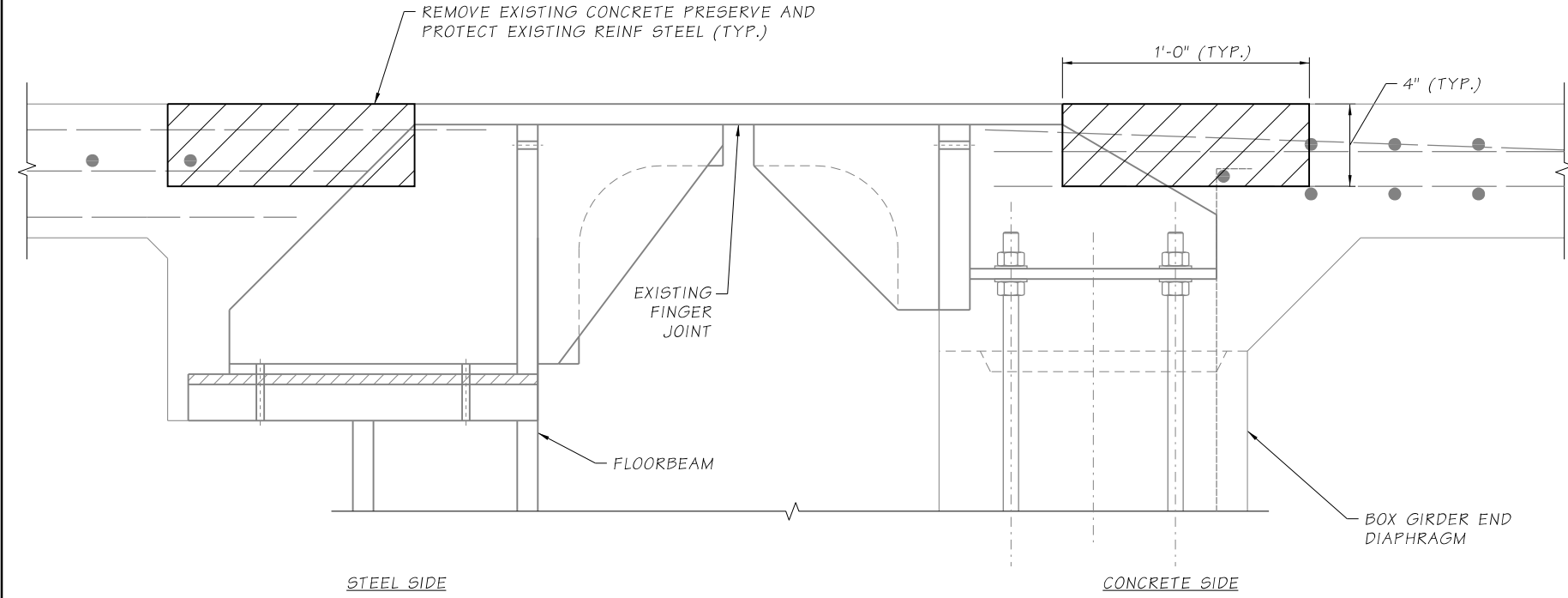
BRIDGE AND STRUCTURES OFFICE



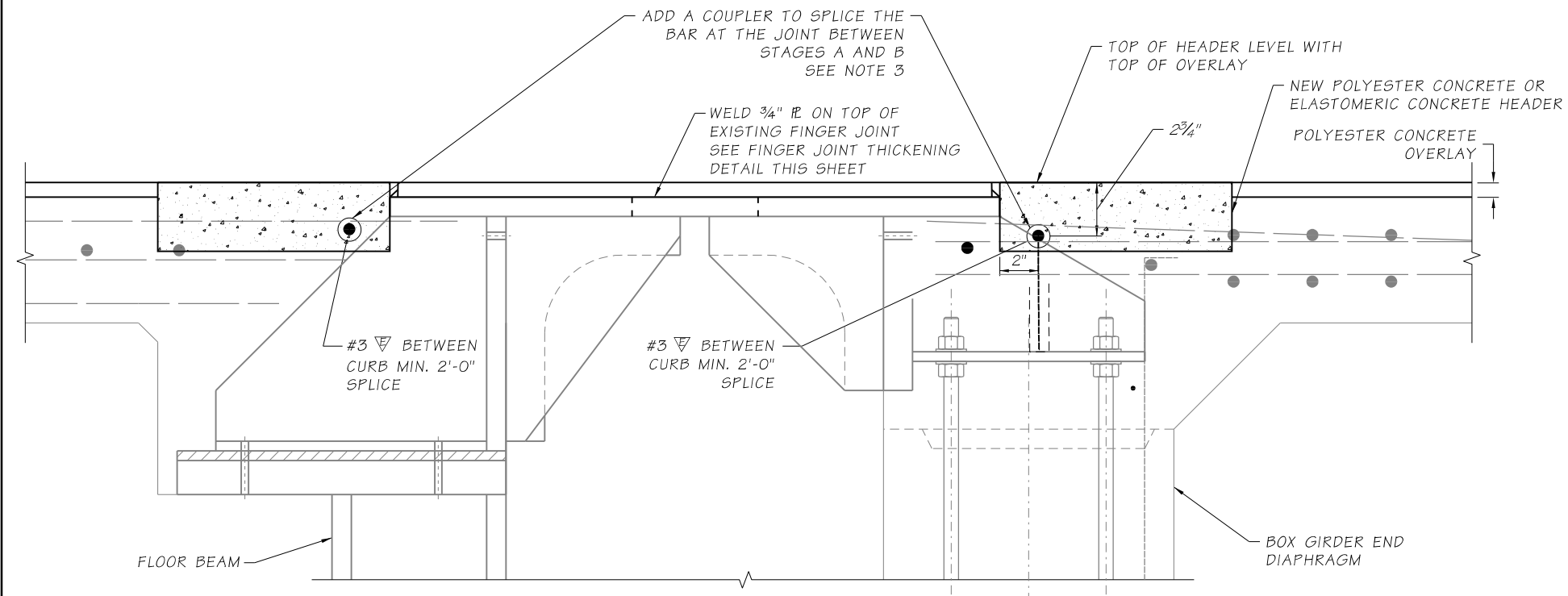
I-5
NB AND SB LEWIS RVR BR
DECK REPAIR / OVERLAY
BRIDGE NO. 5/40E
DECK JOINT SEALANT DETAIL

BRIDGE SHEET NO.
BA8
SHEET 55 OF 84 SHEETS

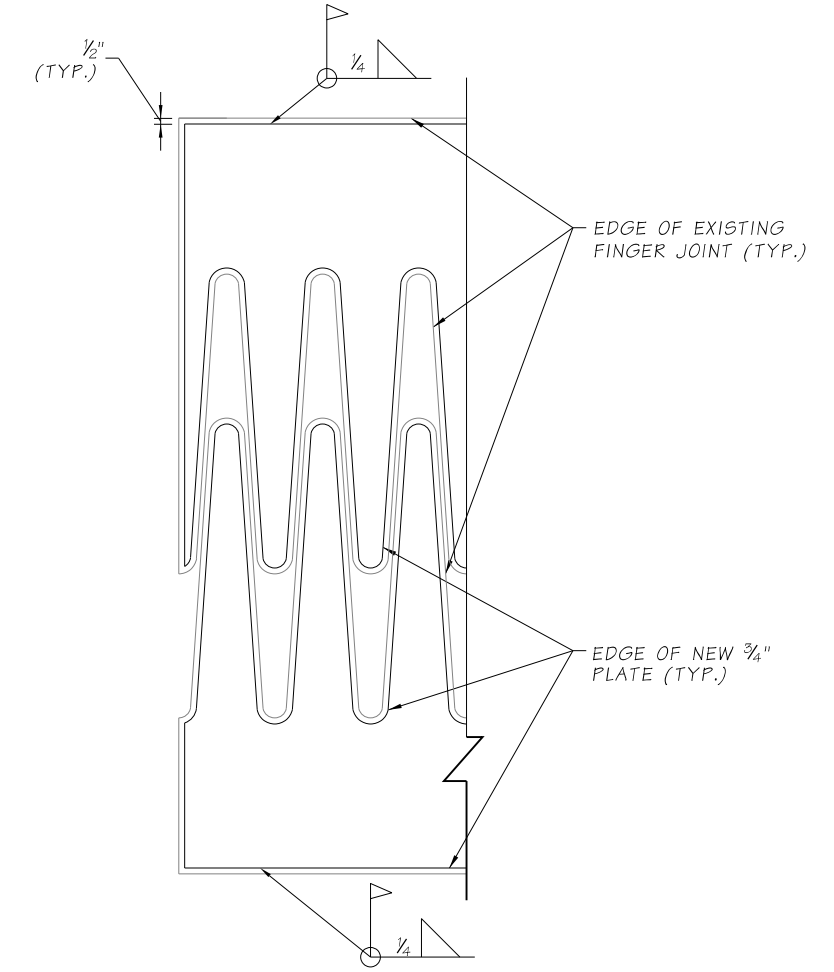
FILE NO. SHEET



FINGER JOINT SECTION @ PIER 3
EXISTING CONDITION






FINGER JOINT SECTION @ PIER 3
MODIFIED CONDITION



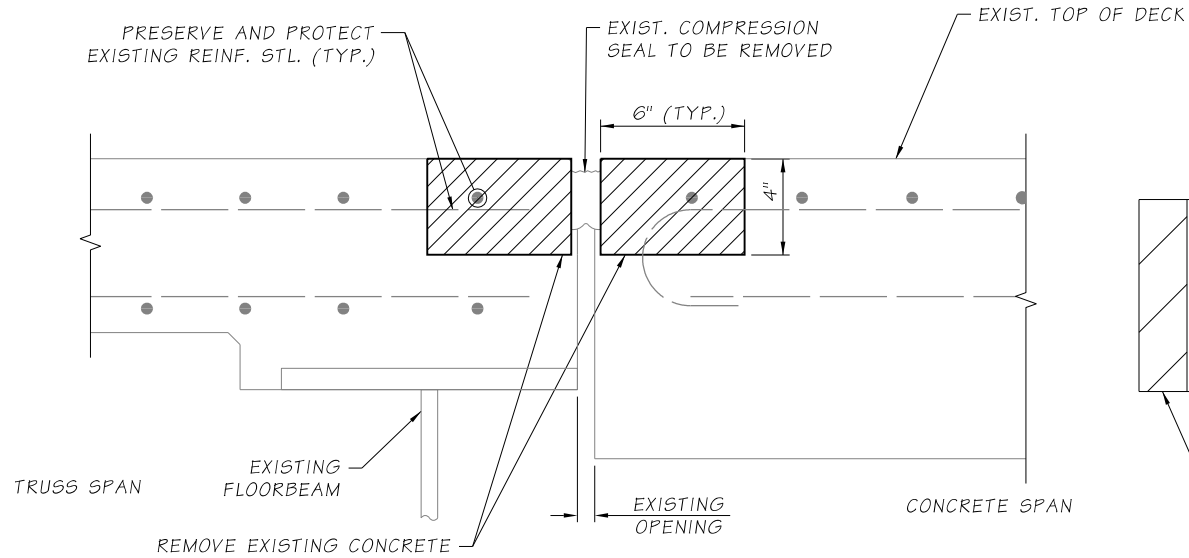
FINGER JOINT THICKENING DETAIL

NOTES:

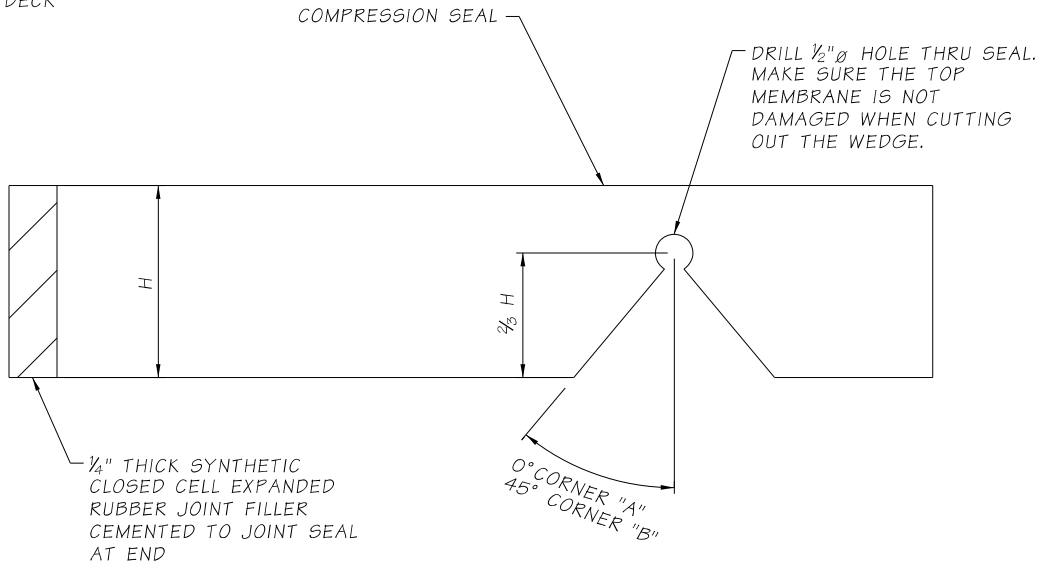
1. NEW STEEL SHALL BE ASTM A573 OR A36.
2. ALL EXISTING STEEL REINFORCEMENT AND STRUCTURAL STEEL SHALL BE PRESERVED. NOTIFY ENGINEER FOR ANY OBSERVED DEFECTS.
3. THE COUPLER SHALL BE CAPABLE OF DEVELOPING THE FULL STRENGTH OF THE BAR. THERE SHALL BE A MINIMUM OF 2" OF CONCRETE COVER OVER THE COUPLER.

FILE NAME X:\211129 WSDOT SW Bridge Deck & Exp\CAD\Bridge 5-40E\IDGN\09 BR5-40E BA9 Expansion Joint Modification Detail at Pier 3.dgn										REGION NO. STATE		FED.AID PROJ.NO. NHFP-0051(316)		BRIDGE AND STRUCTURES OFFICE		 SEE SHEET CT1 DATE		 Washington State Department of Transportation		I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY BRIDGE NO. 5/40E		BRIDGE SHEET NO. BA9	
Bridge Desgn Engr A.LELAND						10 WASH																	
Supervisor						JOB NUMBER 22X333																	
Designed By G.CHEN						CONTRACT NO.																	
Checked By J.LI						LOCATION NO. XL6379, XL6380		SEE SHEET CT1 DATE		 RHC ENGINEERING		EXPANSION JOINT MODIFICATION AT PIER 3 (FINGER JOINT)								SHEET OF 84 SHEETS			
Detailed By T.LOZADA																							
Bridge Projects Engr M.ROSA																							
Prelim. Plan By																							
Architect/Specialist		REVISION		DATE		BY																	

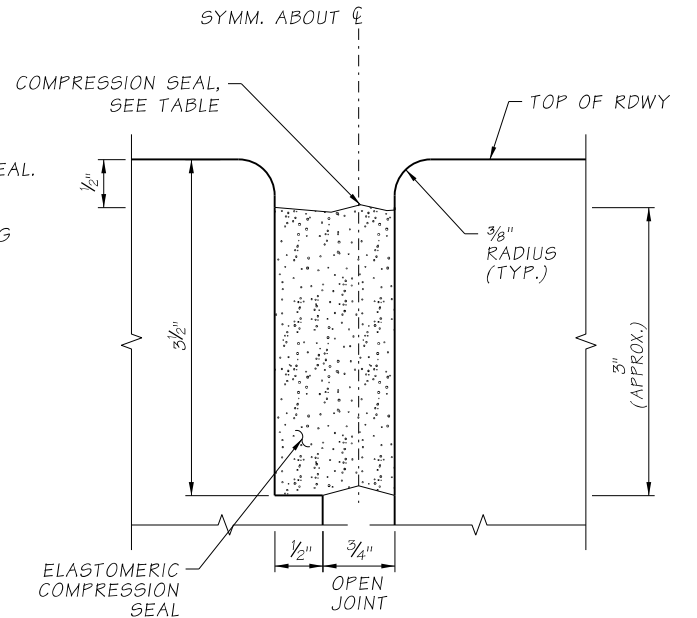
FILE NO. SHEET



JOINT SECTION @ PIER 6
EXISTING CONDITION

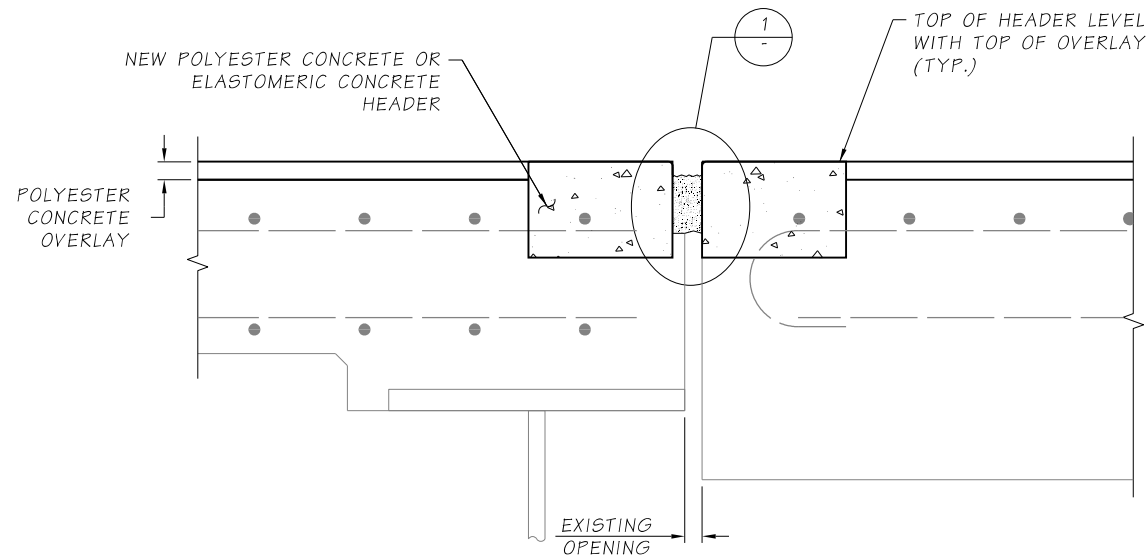


SEAL CUTTING DETAIL

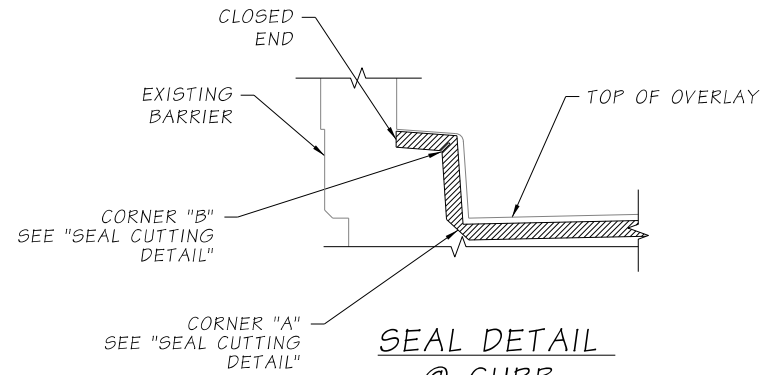


SECTION @ ROADWAY

COMPRESSION SEAL DETAIL 1



JOINT SECTION @ PIER 6
MODIFIED CONDITION






SEAL DETAIL
@ CURB

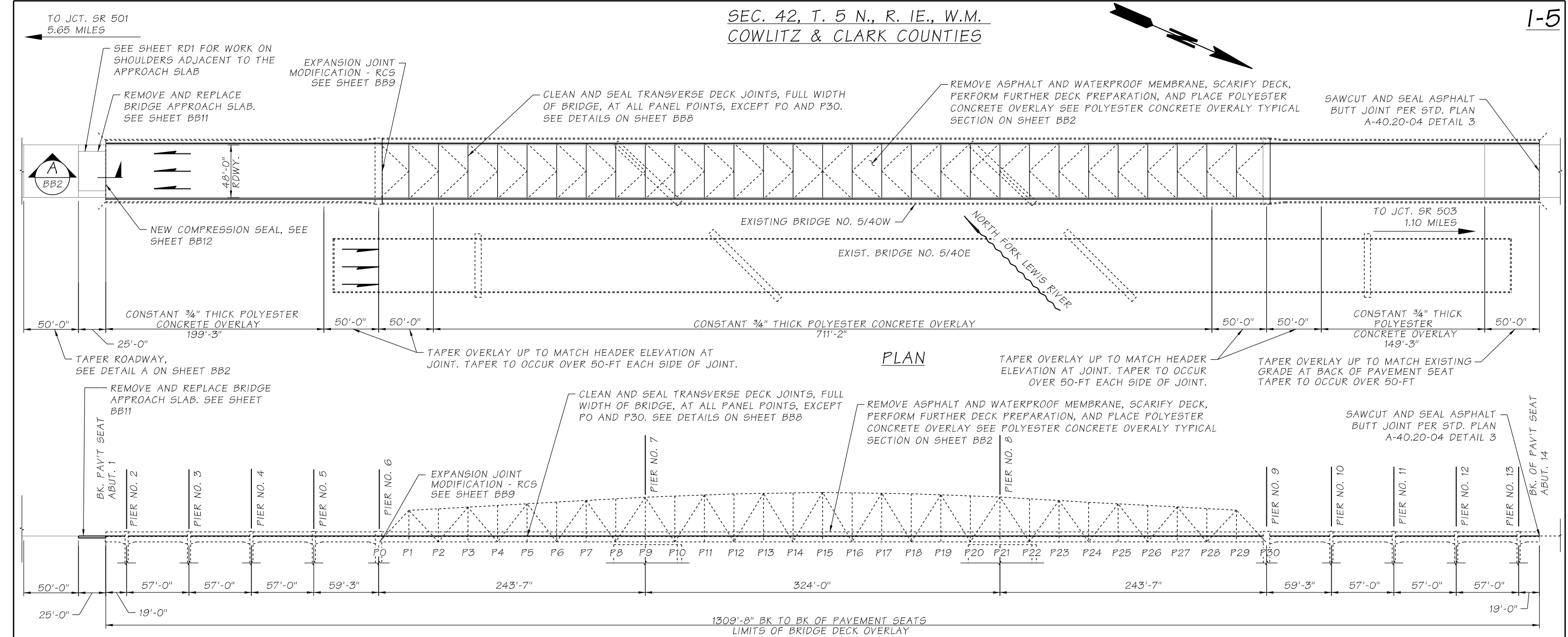
COMPRESSION SEAL JOINT TABLE

D.S. BROWN		WATSON BOWMAN ACME	
SEAL	WIDTH	SEAL	WIDTH
CV-2250	2 1/4"	WA-225	2 1/4"

NOTES:

- ALL EXISTING STEEL REINFORCEMENT AND STRUCTURAL STEEL SHALL BE PRESERVED. NOTIFY ENGINEER FOR ANY OBSERVED DEFECTS.
- TESTING SHALL BE PER ASTM D2628 PRIOR TO USE OF ANY COMPRESSION SEAL.

FILE NAME X:\211129 WSDOT SW Bridge Deck & Expl\CAD\Bridge 5-40E\IDGN\10 BR5-40E BA10 Expansion Joint Modification Detail at Pier 6.dgn										BRIDGE AND STRUCTURES OFFICE		 Washington State Department of Transportation	I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY BRIDGE NO. 5/40E	BRIDGE SHEET NO. BA10
Bridge Desgn Engr	A.LELAND				REGION NO.	STATE	FED.AID PROJ.NO.							
Supervisor					10	WASH	NHFP-0051(316)							
Designed By	G.CHEN				JOB NUMBER									
Checked By	J.LI				22X333									
Detailed By	T.LOZADA				CONTRACT NO.		LOCATION NO.							
Bridge Projects Engr	M.ROSA						XL6379, XL6380							
Prelim. Plan By														
Architect/Specialist		REVISION	DATE	BY						 RHC ENGINEERING	EXPANSION JOINT MODIFICATION AT PIER 6 (COMPRESSION SEAL)	SHEET 57 OF 84 SHEETS		



GENERAL NOTES:

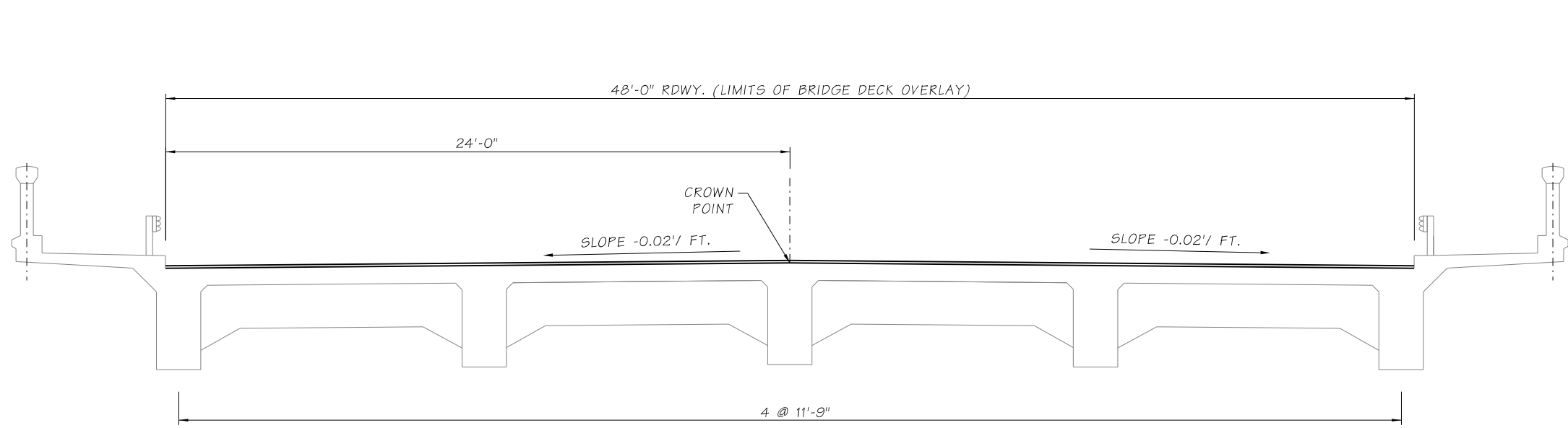
- ALL MATERIAL AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE WASHINGTON STATE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD, BRIDGE, AND MUNICIPAL CONSTRUCTION, 2023.
- THE REHABILITATION DESIGN HAS BEEN IN ACCORDANCE WITH WSDOT BRIDGE DESIGN MANUAL (2022) AND AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS - 9TH EDITION, 2020.
- EXISTING FEATURES AND DIMENSIONS ARE BASED ON INSPECTION REPORTS AND AS-BUILT PLANS. ALL RELEVANT DIMENSIONS AND EXISTING CONDITIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO ORDERING MATERIALS OR STARTING WORK.
- SEE CONSTRUCTION LOAD RESTRICTIONS FOR PAVEMENT REMOVAL ON SHEET BB3.

LEGEND

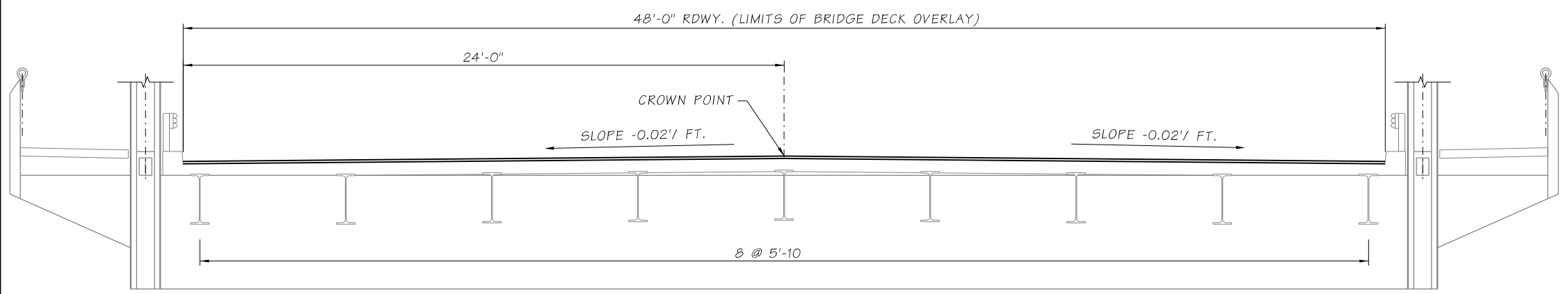
- A
BB2
- IDENTIFIES SECTION OR VIEW
- TAKEN OR SHOWN ON BRIDGE SHEET BB2
- 1
-
- IDENTIFIES DETAIL
- TAKEN OR SHOWN ON THE SAME SHEET

ASPHALT REMOVAL, POLYESTER CONCRETE OVERLAY, JOINT SEALING, AND EXPANSION JOINT MODIFICATION

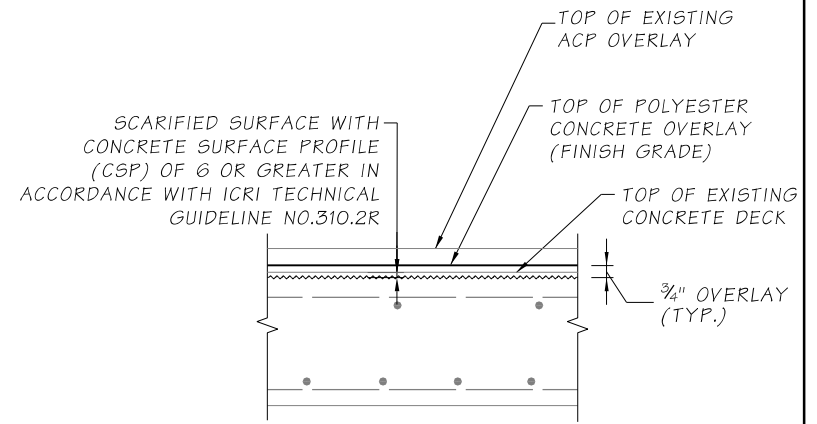
SR



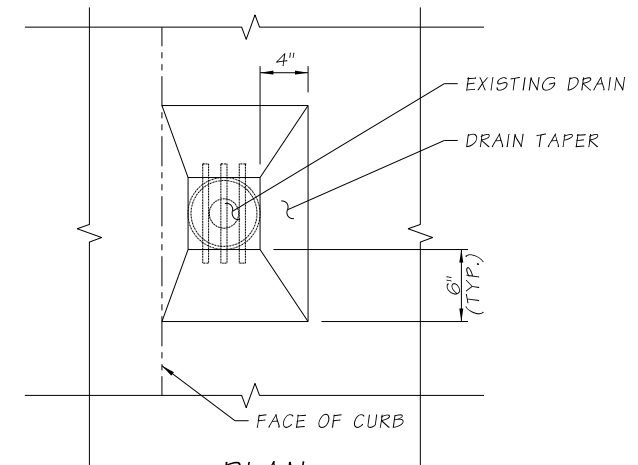
TYPICAL BRIDGE SECTION AT APPROACH SPANS
NORTH AND SOUTH APPROACH SPAN



TYPICAL BRIDGE SECTION AT TRUSS SPANS

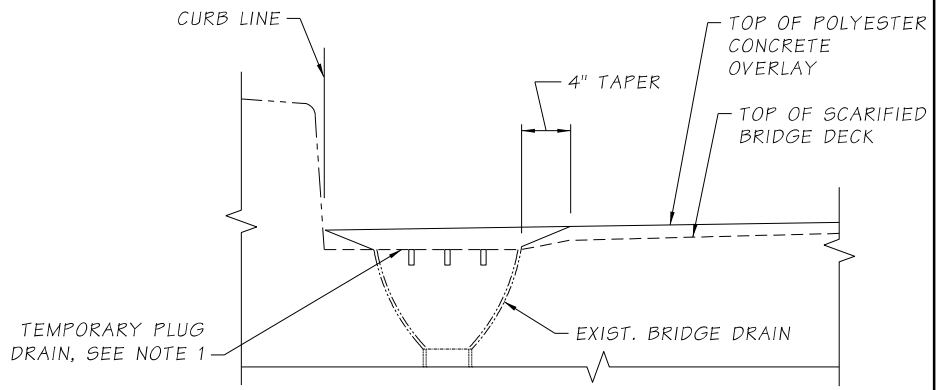
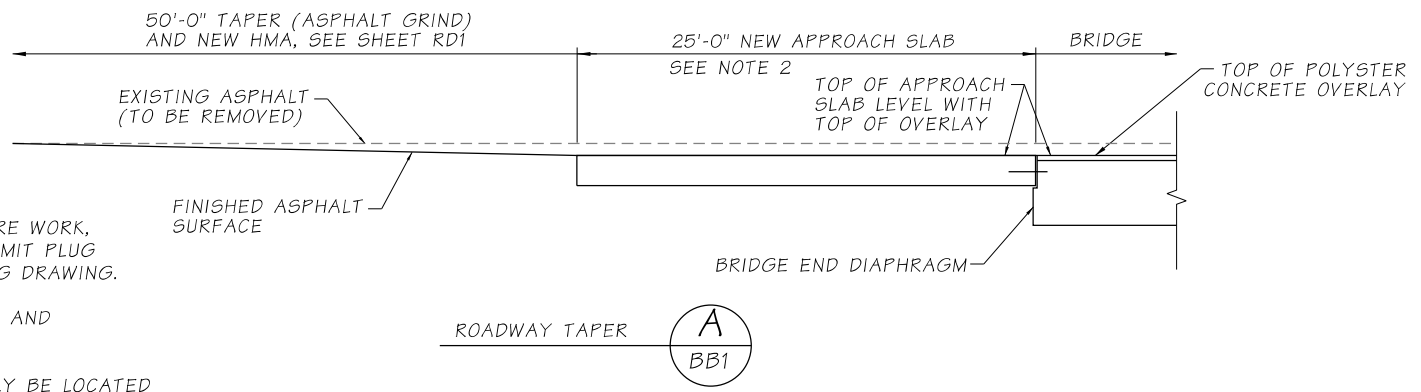


POLYESTER CONCRETE OVERLAY
TYPICAL DETAIL



PLAN
OVERLAY TAPER AT DRAIN

SHOWN FOR RECTANGULAR DRAIN, ROUND DRAIN SEE SECTION BELOW





TYPICAL SECTION
OVERLAY TAPER AT DRAIN

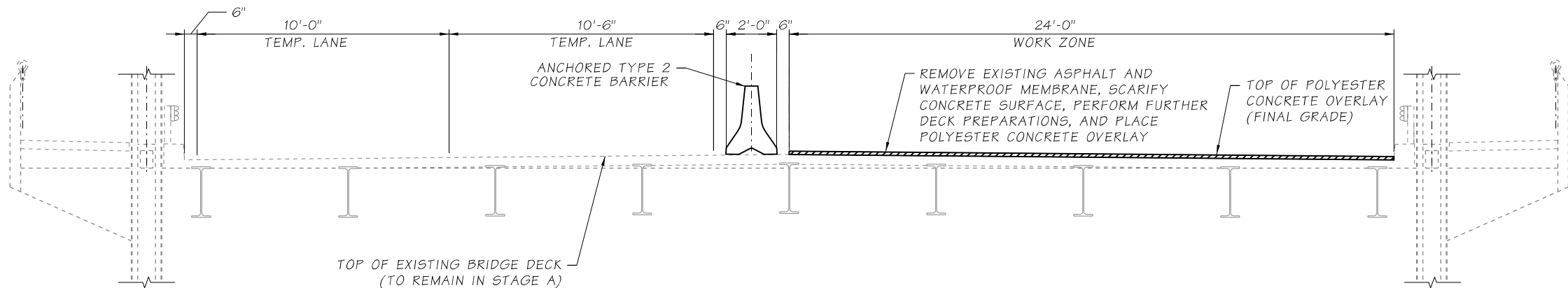
NOTES:

1. TEMPORARILY PLUG EXISTING DRAIN HOLE BEFORE WORK, REMOVE PLUG AFTER OVERLAY PLACEMENT. SUBMIT PLUG DETAIL TO THE ENGINEER AS A TYPE 2 WORKING DRAWING.
2. SEE SHEET BB11 AND BB12 FOR APPROACH SLAB AND EXPANSION ANCHOR DETAILS.
3. LONGITUDINAL JOINTS IN THE OVERLAY MAY ONLY BE LOCATED AT THE CROWN POINT, AT THE LANE LINES, OR AT THE CENTER OF LANES, UNLESS APPROVED OTHERWISE BY THE ENGINEER.

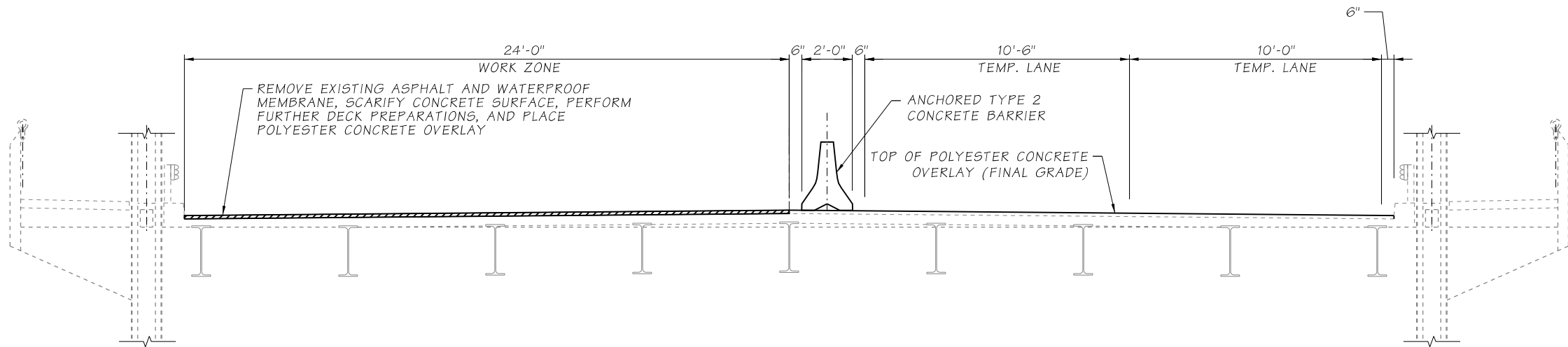
FILE NO. SHEET 59 OF 84 SHEETS

FILE NAME X:\211129 WSDOT SW Bridge Deck & Exp\CAD\Bridge 5-40\WDGN\02 BR5-40W BB2 Typical Section.dgn										BRIDGE AND STRUCTURES OFFICE			I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY BRIDGE NO. 5/40W	BRIDGE SHEET NO. BB2
Bridge Design Engr	A.LELAND				REGION NO.	STATE	FED.AID PROJ.NO. NHFP-0051(316)	LOCATION NO. XL6379, XL6380						
Supervisor					10	WASH								
Designed By	G.CHEN													
Checked By	J.LI					JOB NUMBER	CONTRACT NO.							
Detailed By	T.LOZADA					22X333								
Bridge Projects Engr	M.ROSA													
Prelim. Plan By														
Architect/Specialist		REVISION	DATE	BY										
													TYPICAL SECTION AND POLYESTER OVERLAY TYPICAL DETAILS	SHEET 59 OF 84 SHEETS

SR FILE NO. SHEET






STAGE A
LOOKING NORTH



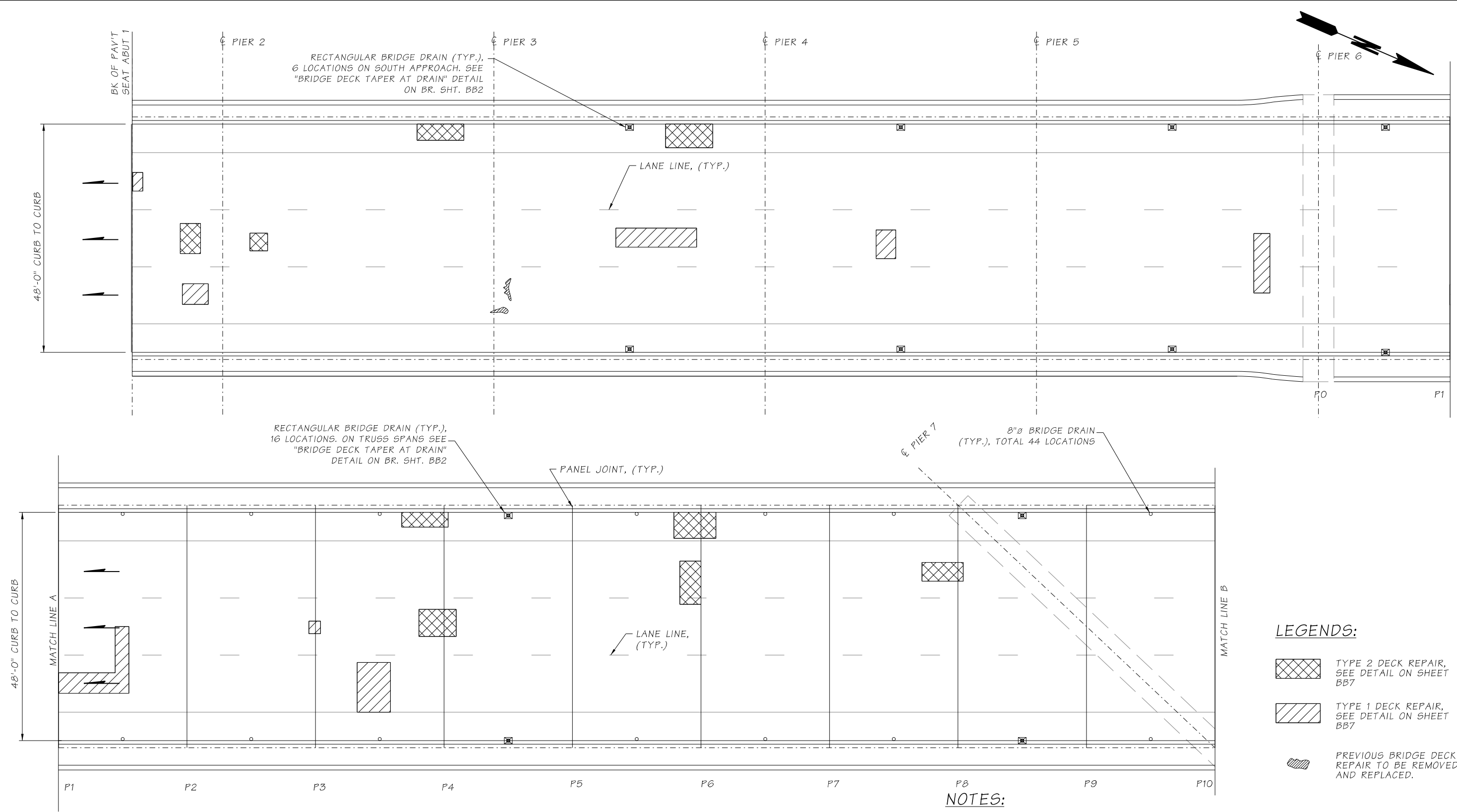
STAGE B
LOOKING NORTH

CONSTRUCTION LOAD LIMITS:


1. TRAFFIC SHALL BE CONFIGURED AS SHOWN ON THIS PLAN SHEET.
2. ONLY ONE LINE OF TEMPORARY CONCRETE BARRIER IS ALLOWED ON THE BRIDGE. THE WEIGHT OF THE TEMPORARY CONCRETE BARRIER SHALL NOT EXCEED 410 PLF.
3. THE MAXIMUM TOTAL CONSTRUCTION LOAD ON THE BRIDGE, INCLUDING EQUIPMENT AND MATERIALS (EXCEPT FOR THE TEMPORARY CONCRETE BARRIER), SHALL NOT EXCEED 54,000 LBS AND SHALL NOT EXCEED CURRENT POSTED LOAD LIMIT FOR THE BRIDGE.
4. THE MAXIMUM TOTAL CONSTRUCTION LOAD TRIBUTARY TO ANY OF THE 62 LOWER PANEL POINTS, INCLUDING EQUIPMENT AND MATERIALS (EXCEPT FOR THE TEMPORARY CONCRETE BARRIER), SHALL NOT EXCEED 27,000 LBS.
5. WORKING DRAWINGS AND CALCULATIONS (TYPE 2E) SHALL BE SUBMITTED IN ACCORDANCE WITH STANDARD SPECIFICATION SECTION 1-05.3. WORKING DRAWINGS AND CALCULATIONS SHALL SHOW THAT ALL OF THE ABOVE CONSTRAINTS HAVE BEEN MET FOR ALL CONSTRUCTION LOADS. CONSTRUCTION LOADS SHALL NOT BE PLACED ON THE BRIDGE UNTIL THE ASSOCIATED WORKING DRAWINGS AND CALCULATIONS HAVE BEEN REVIEWED AND ACCEPTED BY WSDOT.
6. REMOVED MATERIAL, DEBRIS, AND SPENT ABRASIVE BLAST MEDIA SHALL BE COLLECTED AND REMOVED FROM THE BRIDGE DAILY, AT A MINIMUM, TO KEEP UNNECESSARY WEIGHT OFF THE BRIDGE.
7. ALL REMOVED ASPHALT MATERIAL, FOR EACH CONSTRUCTION STAGE, SHALL BE REMOVED FROM THE BRIDGE PRIOR TO PLACING ANY NEW OVERLAY MATERIAL ON THE BRIDGE.
8. SEE PAVEMENT REMOVAL RESTRICTIONS ON SHEET BB10.


FILE NAME X:\211129 WSDOT SW Bridge Deck & Expl\CAD\Bridge 5-40W\IDGN\03 BR5-40W BB3 Construction Sequence.dgn										BRIDGE AND STRUCTURES OFFICE	 <small>SEE SHEET CT1 DATE</small> P.E. STAMP BOX	 Washington State Department of Transportation	 RHC ENGINEERING	I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY BRIDGE NO. 5/40W		BRIDGE SHEET NO. BB3
Bridge Design Engr	A.LELAND				REGION NO. 10	STATE WASH	FED.AID PROJ.NO. NHFP-0051(316)	CONSTRUCTION STAGING AND LOAD LIMITS						SHEET 60 OF 84 SHEETS		
Supervisor					JOB NUMBER 22X333			LOCATION NO. XL6379, XL6380								
Designed By	J.LI				CONTRACT NO.											
Checked By	G.CHEN															
Detalled By	T.LOZADA															
Bridge Projects Engr	M.ROSA															
Prelim. Plan By																
Architect/Specialist		REVISION	DATE	BY												


SR FILE NO. SHEET



LEGENDS:




 TYPE 2 DECK REPAIR, SEE DETAIL ON SHEET BB7

 TYPE 1 DECK REPAIR, SEE DETAIL ON SHEET BB7

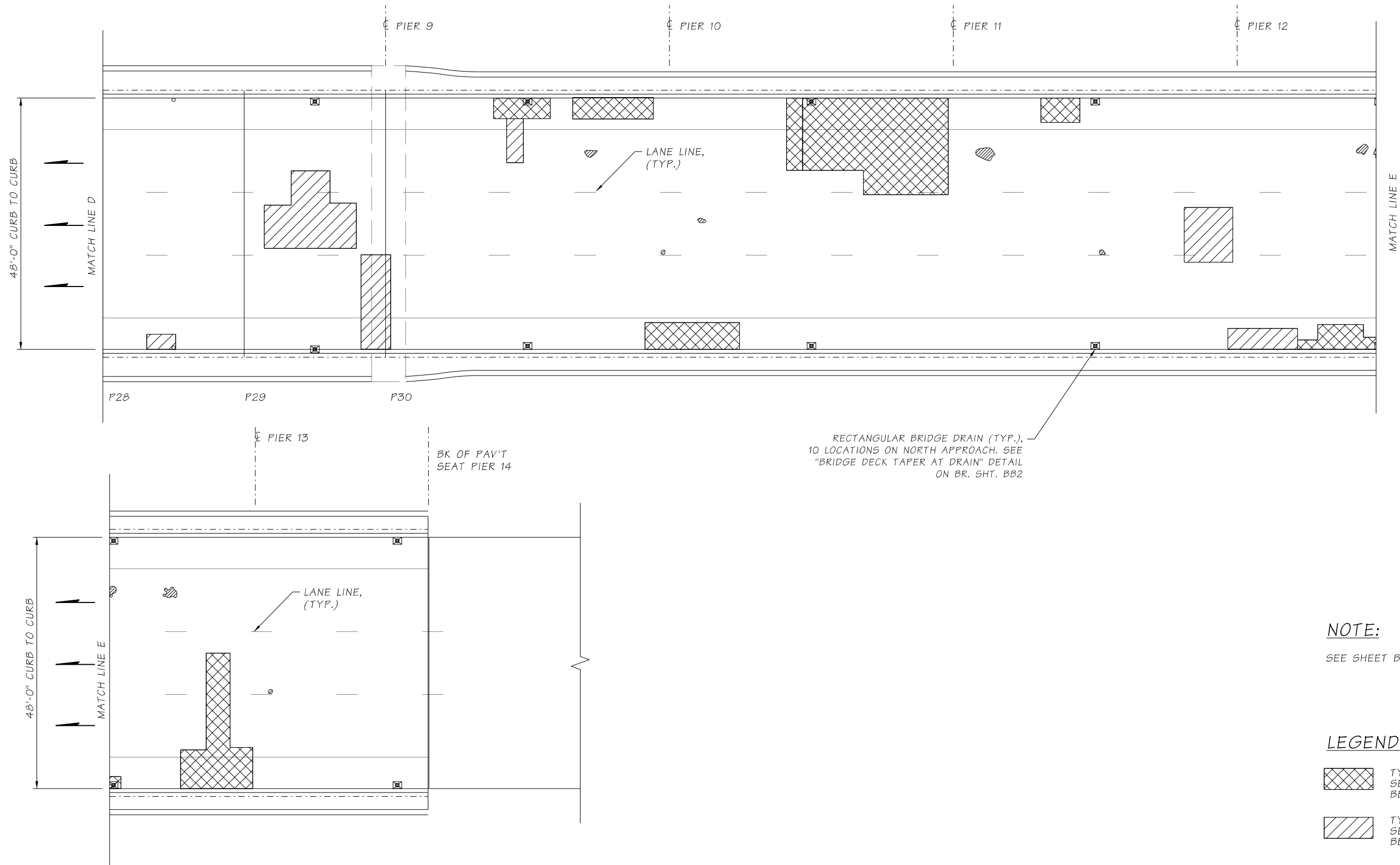
 PREVIOUS BRIDGE DECK REPAIR TO BE REMOVED AND REPLACED.

- NOTES:**
- ALL BRIDGE DECK REPAIRS SHOWN ARE APPROXIMATE LOCATIONS ONLY. THE CONTRACTOR SHALL PERFORM BRIDGE DECK REPAIRS AS DIRECTED BY THE ENGINEER.
 - SEE BRIDGE SHEET BB8 FOR PANEL JOINT DETAILS.
 - SEE SHEET BB7 FOR REINFORCING STEEL REPAIR REQUIREMENTS AND DETAILS.

**PLAN
FURTHER DECK PREPARATION**

FILE NAME X:\211129 WSDOT SW Bridge Deck & Exp\CAD\Bridge 5-40W\DGNI\04-06 BR5-40W BB4-BB6 Deck Preparation Plan Details 1-3.dgn										<div>BRIDGE AND STRUCTURES OFFICE</div>		<div> SEE SHEET CT1 P.E. STAMP BOX DATE</div>	<div> Washington State Department of Transportation</div>	<div>I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY BRIDGE NO. 5/40W</div>	<div>BRIDGE SHEET NO. BB4</div>				
Bridge Desgn Engr	A.LELAND																		
Supervisor																	<div> RHC ENGINEERING</div>	<div>FURTHER DECK PREPARATION PLAN 1 OF 3</div>	<div>SHEET 61 OF 84 SHEETS</div>
Designed By	G.CHEN																		
Checked By	J.LI																		
Detailed By	Y.WANG																		
Bridge Projects Engr	M.ROSA																		
Prelim. Plan By																			
Architect/Specialist		REVISION	DATE	BY	REGION NO. 10	STATE WASH	FED.AID PROJ.NO. NHFP-0051(316)	LOCATION NO. XL6379, XL6380											

SR FILE NO. SHEET




PLAN
FURTHER DECK PREPARATION

NOTE:

SEE SHEET BB4 FOR REPAIR NOTES.



LEGENDS:

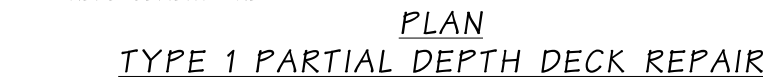
 TYPE 2 DECK REPAIR, SEE DETAIL ON SHEET BB7

 TYPE 1 DECK REPAIR, SEE DETAIL ON SHEET BB7

 PREVIOUS BRIDGE DECK REPAIR TO BE REMOVED AND REPLACED.

FILE NAME X:\211129 WSDOT SW Bridge Deck & Expl\CAD\Bridge 5-40\WDGN\04-06 BR5-40W BB4-BB6 Deck Preparation Plan Details 1-3.dgn									
Bridge Desgn Engr	A.LELAND				REGION NO.	STATE	FED.AID PROJ.NO.		
Supervisor					10	WASH	NHFP-0051(316)		
Designed By	G.CHEN				JOB NUMBER				
Checked By	J.LI				22X333				
Detailed By	Y.WANG				CONTRACT NO.			LOCATION NO.	
Bridge Projects Engr	M.ROSA							XL6379, XL6380	
Prelim. Plan By									
Architect/Specialist		REVISION	DATE	BY					

BRIDGE AND STRUCTURES OFFICE		 Washington State Department of Transportation	I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY		BRIDGE SHEET NO. BB6
			BRIDGE NO. 5/40W		SHEET 63 OF 84 SHEETS
			FURTHER DECK PREPARATION PLAN 3 OF 3		



LEGEND:

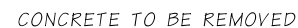


TABLE 1 - ALLOWABLE BAR
DIAMETER AND BAR LENGTH

Diagram illustrating the repair of a lap splice bar. The diagram shows a cross-section of a bridge deck with multiple reinforcement bars. A specific bar is highlighted with a defect, labeled "EXISTING BAR WITH DEFECT". The repair process involves scarifying the concrete surface, labeled "SCARIFIED CONCRETE SURFACE". The top of the existing bridge deck is indicated by a horizontal line, labeled "TOP OF EXISTING BRIDGE DECK". A dimension of $\frac{3}{4}$ " is shown for the depth of the repair. The label "LAP SPLICE BAR" points to the area of the repair. The word "BAR" is partially visible at the bottom left.

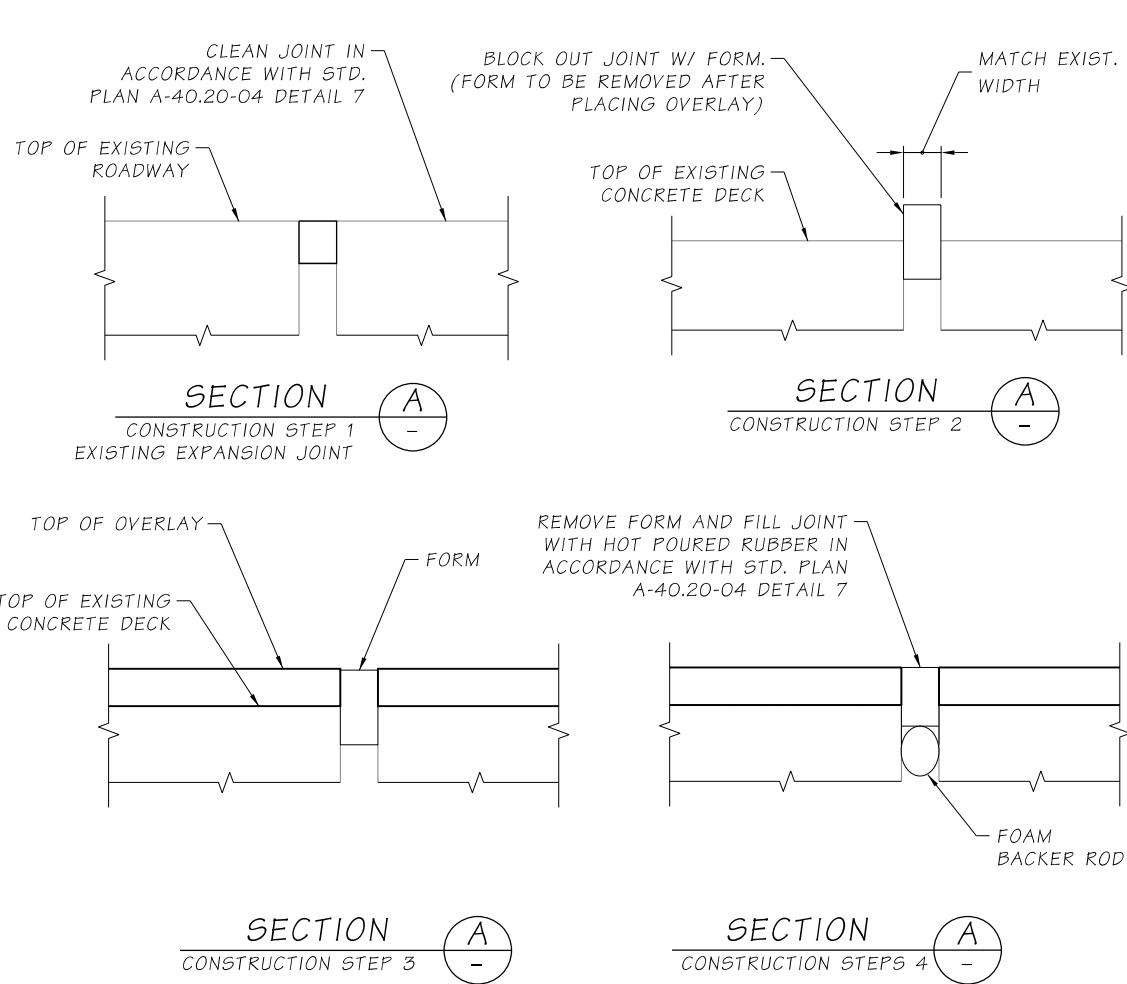
REMOVE ADDITIONAL
CONCRETE AS REQUIRED TO
ACHIEVE A MINIMUM OF $\frac{3}{4}$ "
CLEARANCE AROUND BARS

REBAR REPAIR SECTION

- REINFORCING STEEL REPAIR NOTES:

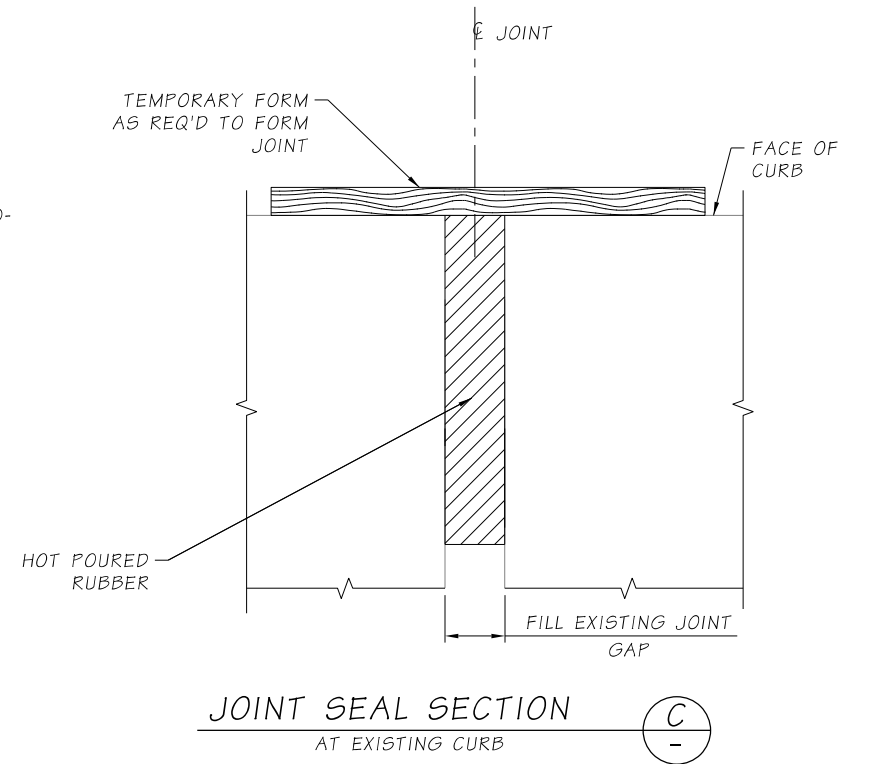
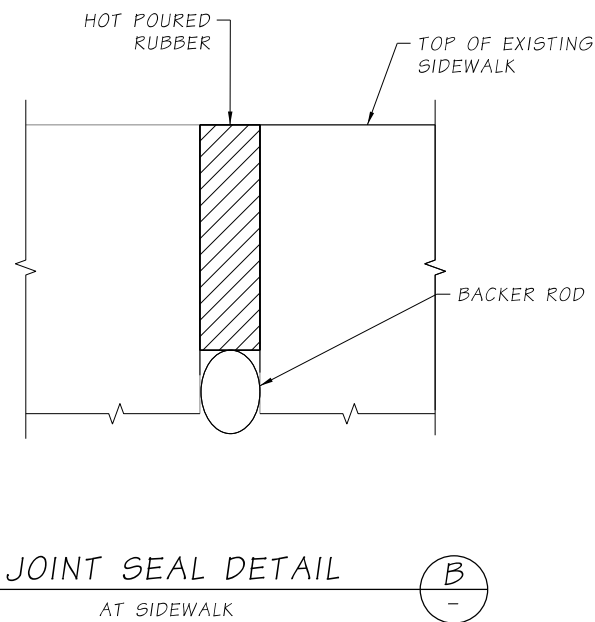
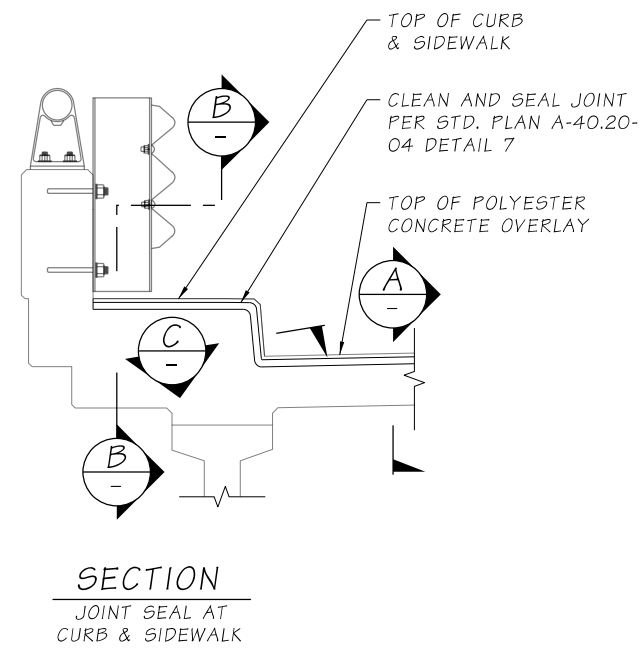
1. REINFORCEMENT BAR REPAIR: FOR ANY REINFORCEMENT BAR WITH GREATER THAN 20% SECTION LOSS OR IS OTHERWISE DAMAGED, PLACE NEW EPOXY-COATED REINFORCEMENT BAR OF THE SAME SIZE AS THE ORIGINAL BAR ADJACENT AND PARALLEL TO THE DEFICIENT BAR.
2. ALL REINFORCING STEEL USED IN REPAIRS SHALL CONFORM TO ASTM A706 GRADE 60.
3. REINFORCING STEEL CLEAR COVER SHALL MATCH EXISTING.

FILE NO. SHEET



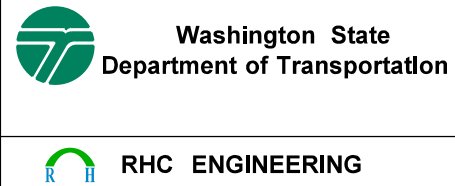
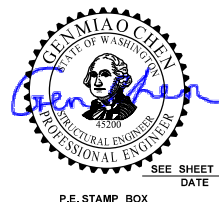
SUGGESTED CONSTRUCTION SEQUENCE:

- STEP 1. CLEAN JOINT
- STEP 2. PLACE FORM IN EXISTING JOINT OPENING.
- STEP 3. PLACE POLYESTER CONCRETE OVERLAY TO FINAL RDWY. ELEVATION.
- STEP 4. REMOVE FORM FROM JOINT OPENING AND FILL WITH HOT POURED RUBBER.



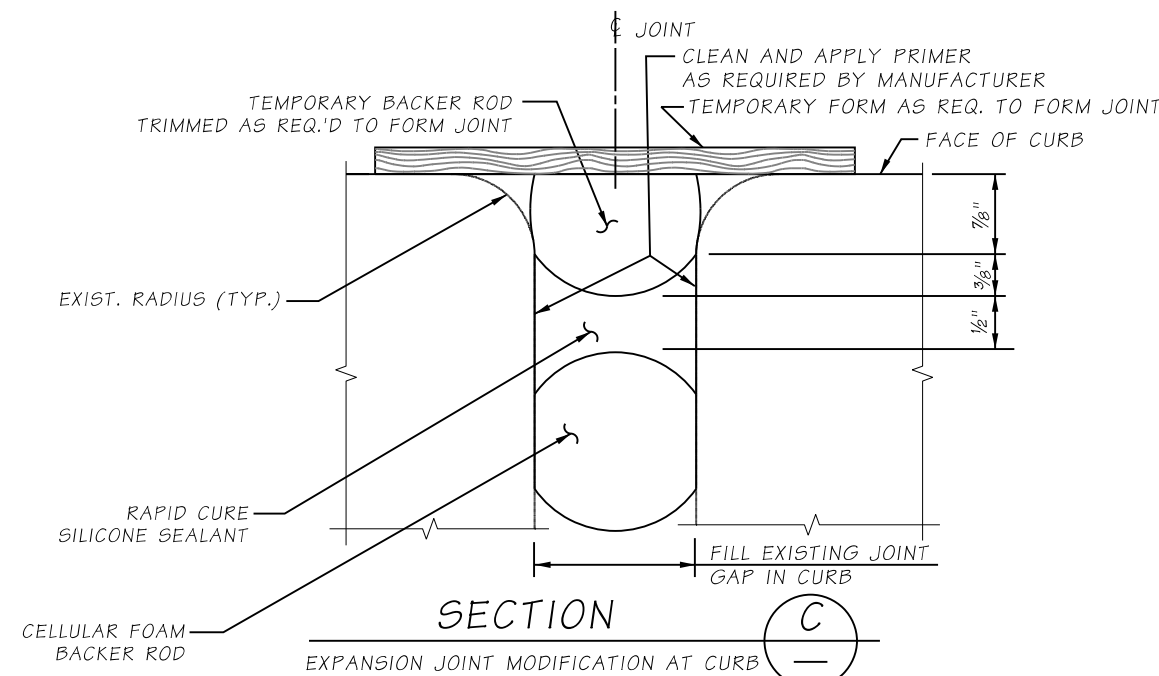
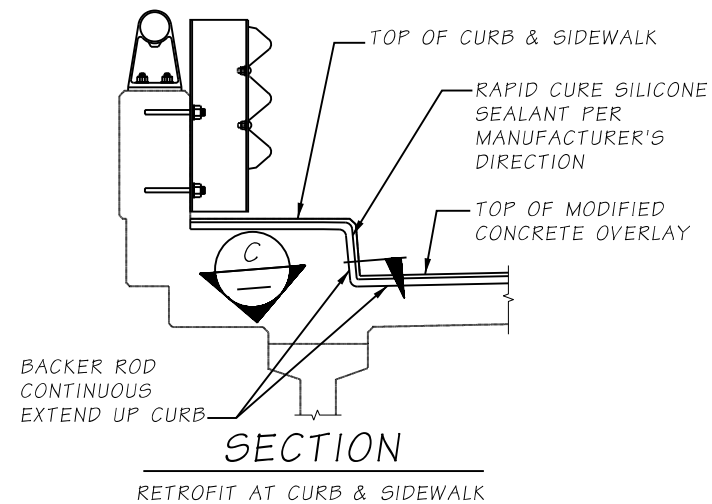
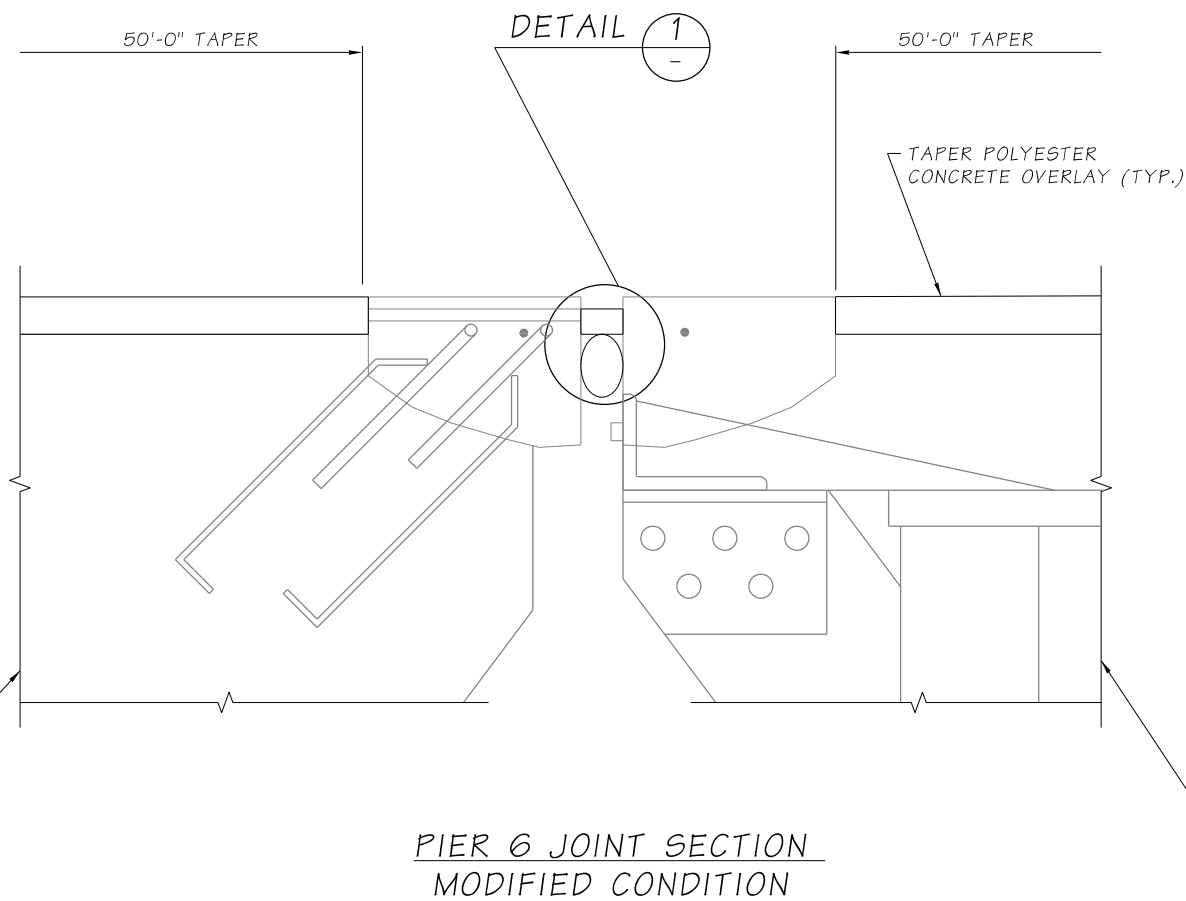
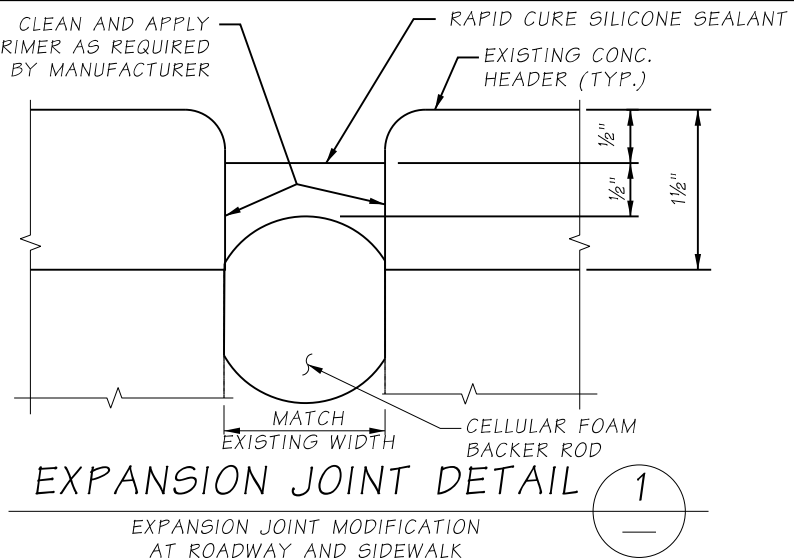
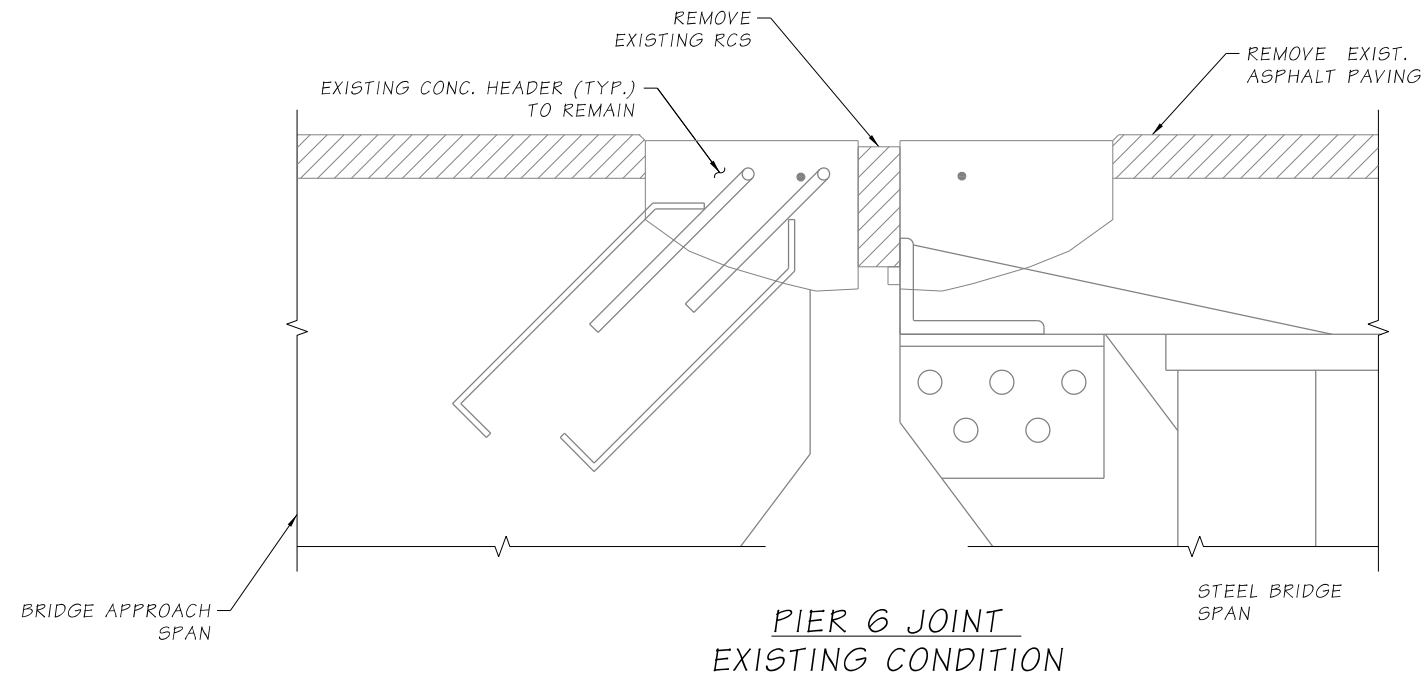
FILE NAME X:\211129 WSDOT SW Bridge Deck & Exp\CAD\Bridge 5-40\WDGN\08 BR5-40W BB8 Seal Joint Modification Details.dgn				REGION NO.	STATE	FED.AID PROJ.NO.	
Bridge Desgn Engr	A.LELAND			10	WASH	NHFP-0051(316)	
Supervisor							
Designed By	G.CHEN						
Checked By	J.LI						
Detailed By	T.LOZADA						
Bridge Projects Engr	M.ROSA						
Prelim. Plan By							
Architect/Specialist							
REVISION		DATE	BY	JOB NUMBER		LOCATION NO.	
				22X333		XL6379, XL6380	
				CONTRACT NO.			

BRIDGE AND STRUCTURES OFFICE



I-5
NB AND SB LEWIS RVR BR
DECK REPAIR / OVERLAY
BRIDGE NO. 5/40W
DECK JOINT SEALANT DETAIL

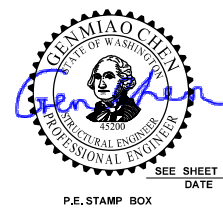
BRIDGE SHEET NO.
BB8
SHEET 65 OF 84 SHEETS



SR
FILE NO.
SHEET

FILE NAME X:\211129 WSDOT SW Bridge Deck & Exp\CAD\Bridge 5-40\WDGN\09 BR5-40W BB9 ExpansionJoint Modification Details at Pier 6.dgn					REGION NO.		STATE		FED.AID PROJ.NO.	
Bridge Desgn Engr	A.LELAND				10		WASH		NHFP-0051(316)	
Supervisor										
Designed By	J.LI									
Checked By	G.CHEN									
Detailed By	T.LOZADA									
Bridge Projects Engr	M.ROSA									
Prelim. Plan By										
Architect/Specialist										
REVISION					DATE		BY		LOCATION NO.	
									XL6379, XL6380	

BRIDGE
AND
STRUCTURES
OFFICE



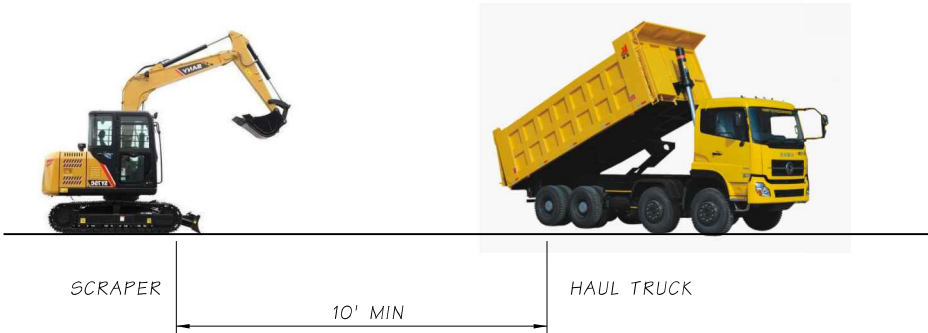
I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY BRIDGE NO. 5/40W		BRIDGE SHEET NO. BB9
EXPANSION JOINT MODIFICATION AT PIER 6		SHEET 66 OF 84 SHEETS

BRIDGE PAVEMENT REMOVAL NOTES:

1.

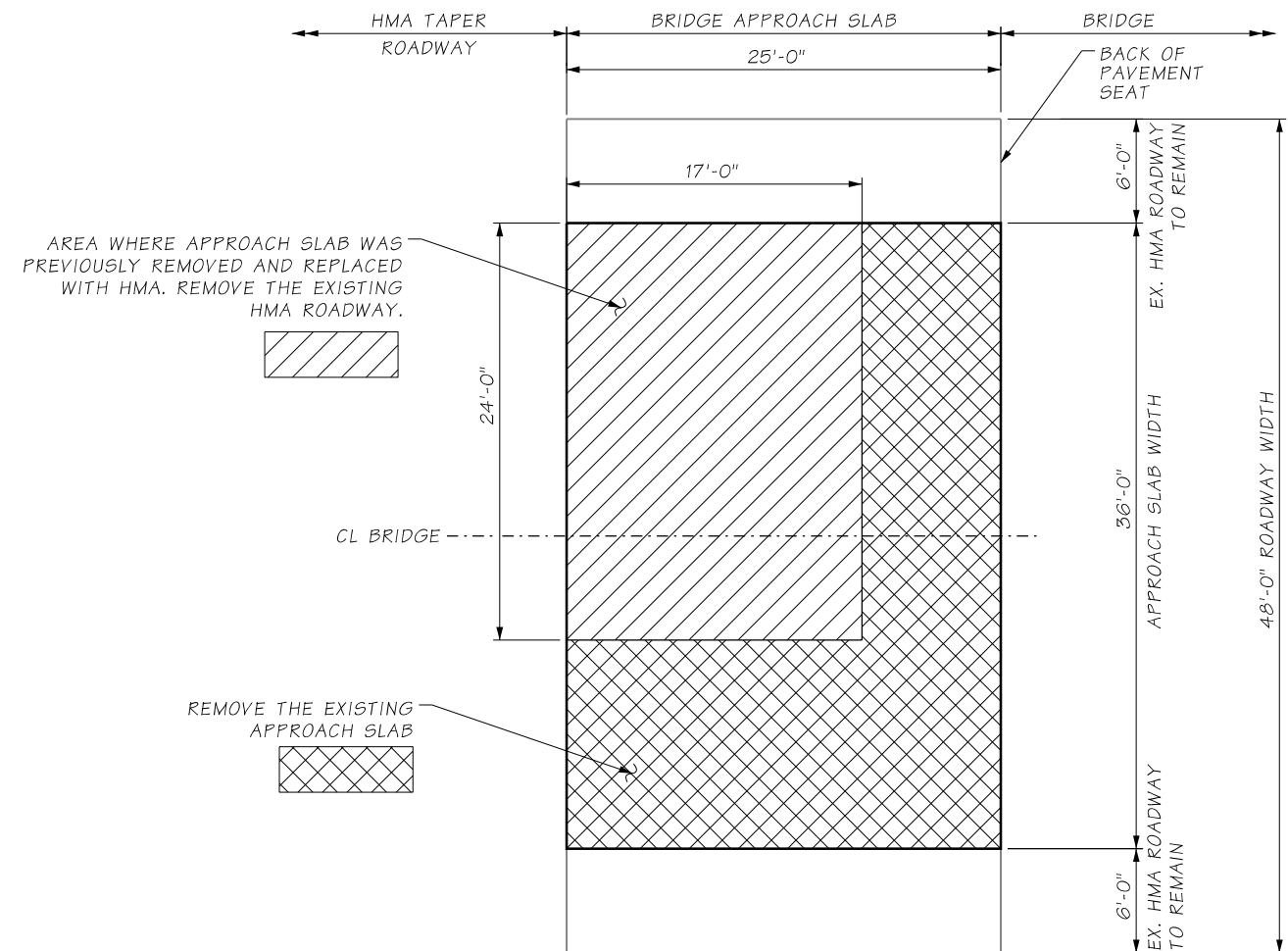
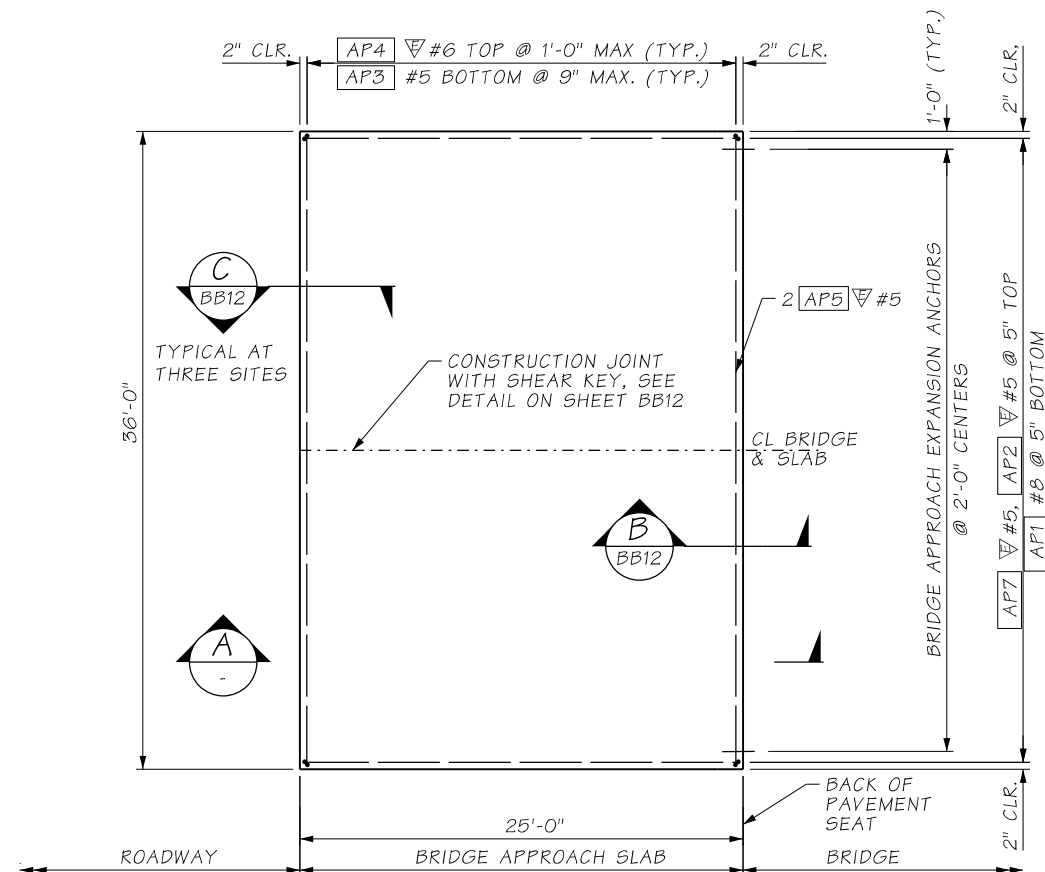
DURING BUTIMUNOUS PAVEMENT REMOVAL WORK ON A BRIDGE, CONSTRUCTION LOADS ON THE BRIDGE SHALL BE RESTRICTED TO THE SPECIFIED BITUMINOUS PAVEMENT REMOVAL TRAIN ONLY ONE REMOVAL TRAIN IS ALLOWED ON A SPAN AT A TIME.
2.

ALL SPECIFIED MAXIMUM GROSS WEIGHTS ARE THE EQUIPMENTS FULLY LOADED WEIGHT.



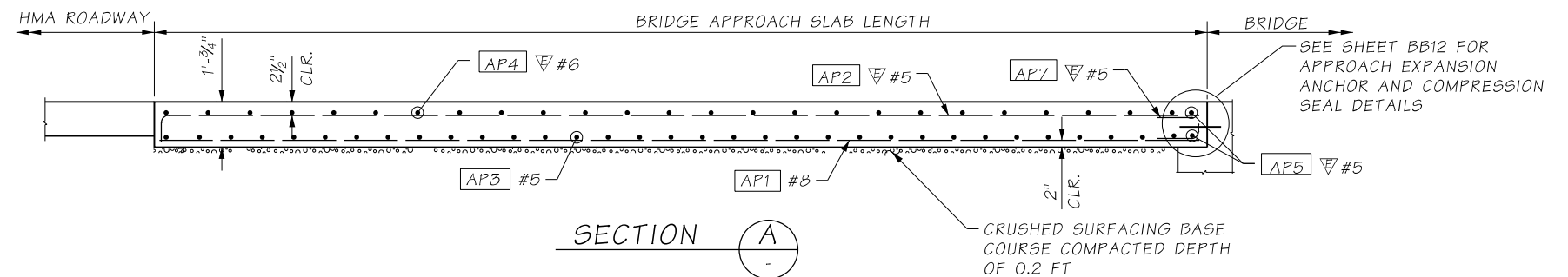
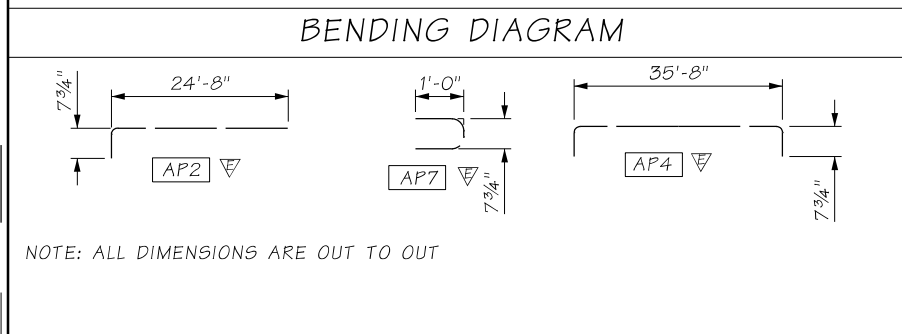
BRIDGE NO.	SCRAPER	HAUL TRUCK
	MAX. GROSS WT. (LBS)	MAX. GROSS WT. (LBS)
5/40W	11,000	20,000




PAVEMENT REMOVAL TRAIN - SCRAPER

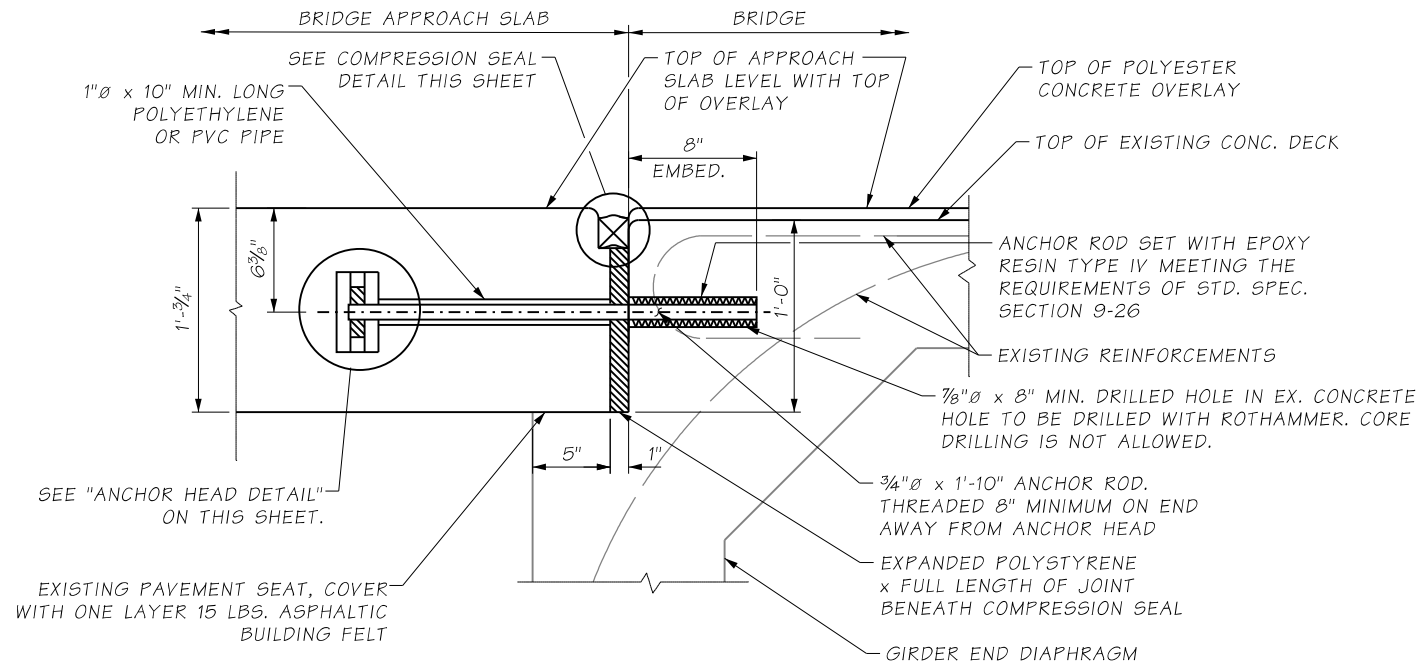


NOTES:

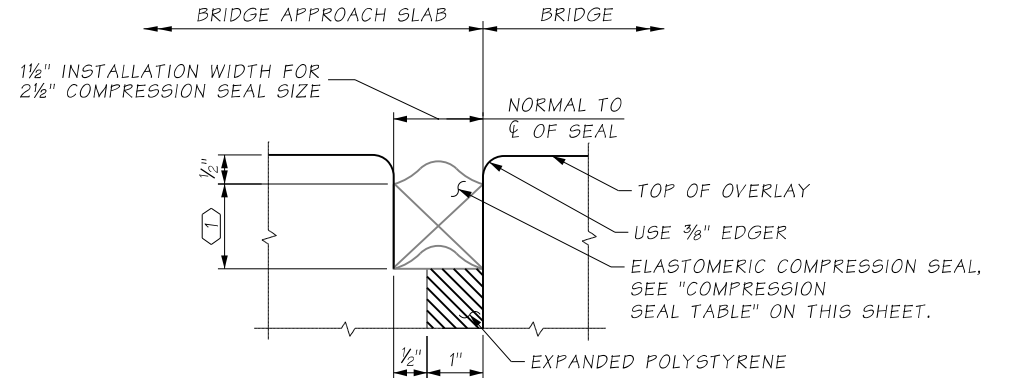
1. ALL EDGES OF BRIDGE APPROACH SLAB SHALL HAVE 1/2" RADIUS.
2. THE MINIMUM LAP SPLICE OF #5 IS 2'-0", ▽ #5 IS 2'-6", ▽ #6 IS 3'-0", AND #8 IS 3'-3". ALL LAP SPLICES SHALL BE STAGGERED SO THAT NO MORE THAN 50% OF REBAR IS SPLICED AT THE SAME LOCATION. LAP SPLICES SHALL BE LOCATED WITHIN THE MIDDLE HALF OF THE BRIDGE APPROACH SLAB. OPTIONAL SPLICES ARE ALLOWED FOR AP4 ▽ #6.
3. SEE SHEET RD1 FOR WORK ON SHOULDERS THAT ARE ADJACENT TO THE APPROACH SLAB



FILE NAME X:\211129 WSDOT SW Bridge Deck & Exp\CAD\Bridge 5-40\WDGN\11 BR5-40W BB11 Bridge Approach Slab Details-1.dgn										<div>BRIDGE AND STRUCTURES OFFICE</div> <div> SEE SHEET CT1 DATE</div> <div>P.E. STAMP BOX</div>		<div> Washington State Department of Transportation</div> <div> RHC ENGINEERING</div>		<div>I-5</div> <div>NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY</div> <div>BRIDGE NO. 5/40W</div>		BRIDGE SHEET NO.
Supervisor				REGION NO. 10 STATE WASH		FED.AID PROJ.NO. NHFP-0051(316)		BB11								
Designed By G.CHEN						JOB NUMBER 22X333								SHEET 68 OF 84 SHEETS		
Checked By J.LI						CONTRACT NO.		LOCATION NO. XL6379, XL6380								
Detailed By Y.WANG																
Bridge Projects Engr M.ROSA																
Prelim. Plan By																
Architect/Specialist		REVISION		DATE		BY										



APPROACH EXPANSION ANCHOR (B)
BB11



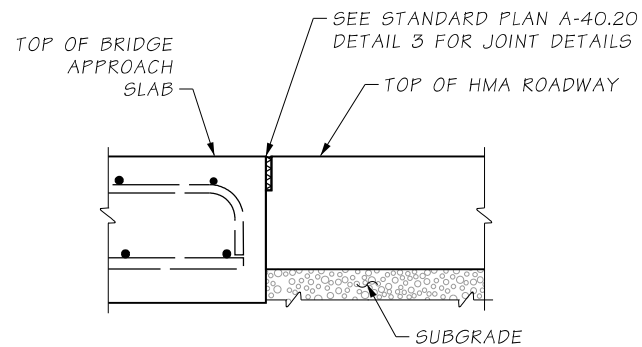
COMPRESSION SEAL DETAIL
EXPANSION JOINT AT BACK OF PAVEMENT SEAT

COMPRESSION SEAL TABLE

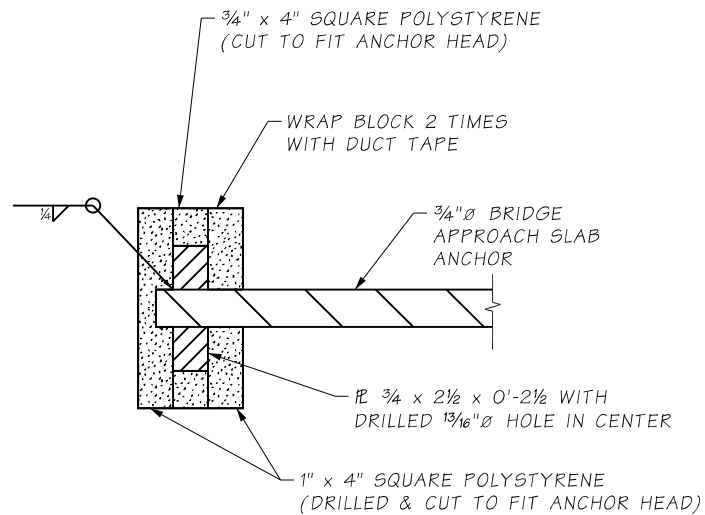
D.S. BROWN		WATSON BOWMAN ACME	
SEAL	WIDTH	SEAL	WIDTH
CV-2502	2 1/2"	WA-250	2 1/2"

TESTING SHALL BE PER ASTM D 2628 PRIOR TO USE.

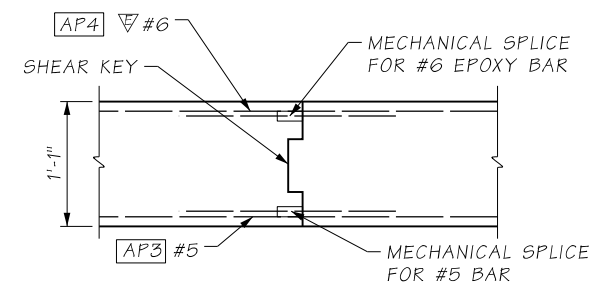
(1.) FULLY COMPRESSED SEAL HEIGHT, SEAL HEIGHT VARIES WITH MANUFACTURER, VERIFY PRIOR TO SLAB CONSTRUCTION



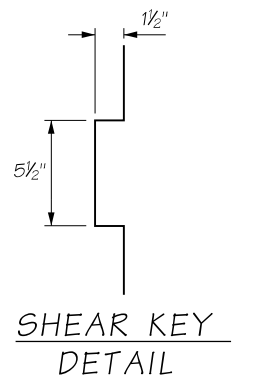
HMA ROADWAY JOINT DETAIL (C)
BB11



ANCHOR HEAD DETAIL



CONSTRUCTION JOINT DETAIL



SHEAR KEY DETAIL

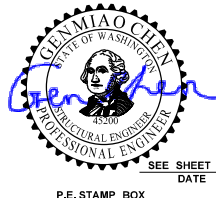
NOTES:

- ALL METAL PARTS OF THE APPROACH EXPANSION ANCHOR SHALL RECEIVE ONE COAT OF PAINT CONFORMING TO STANDARD SPECIFICATION SECTION 9-08.1(2)F OR BE GALVANIZED IN ACCORDANCE WITH AASHTO M 232.
- BRIDGE APPROACH SLAB ANCHORS SHALL BE INSTALLED PARALLEL TO ROADWAY AND TO EACH OTHER.
- MECHANICAL SPLICE CAN BE DAYTON D200 SERIES LOCK COUPLER OR D300 SERIES STANDARD COUPLER, OR OTHER EQUIVALENT, SUBMIT SHOP DRAWINGS AND MANUFACTURE PRODUCT CERTIFICATE OF COMPLIANCE FOR ENGINEER'S APPROVAL. THE COUPLER FOR TOP BAR SHALL HAVE EPOXY FINISH.

SR FILE NO. SHEET

FILE NAME X:\211129 WSDOT SW Bridge Deck & Exp\CAD\Bridge 5-40\WDGN\12 BR5-40W BB12 Bridge Approach Slab Details-2.dgn									
Bridge Design Engr	A.LELAND					REGION NO.	STATE	FED.AID PROJ.NO. NHFP-0051(316)	
Supervisor						10	WASH		
Designed By	G.CHEN					JOB NUMBER 22X333		LOCATION NO. XL6379, XL6380	
Checked By	J.LI								
Detailed By	Y.WANG								
Bridge Projects Engr	M.ROSA					CONTRACT NO.			
Prelim. Plan By									
Architect/Specialist		REVISION			DATE	BY			

BRIDGE
AND
STRUCTURES
OFFICE



Washington State
Department of Transportation



RHC ENGINEERING

I-5
NB AND SB LEWIS RVR BR
DECK REPAIR / OVERLAY

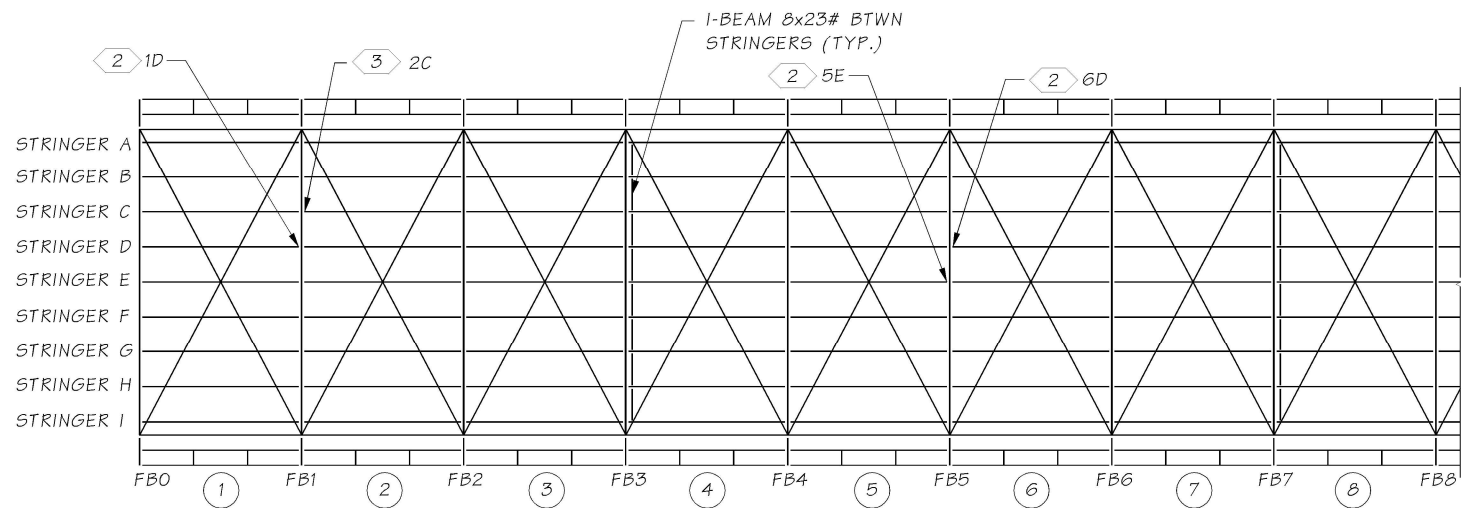
BRIDGE NO. 5/40W

SOUTH END APPROACH SLAB
DETAILS

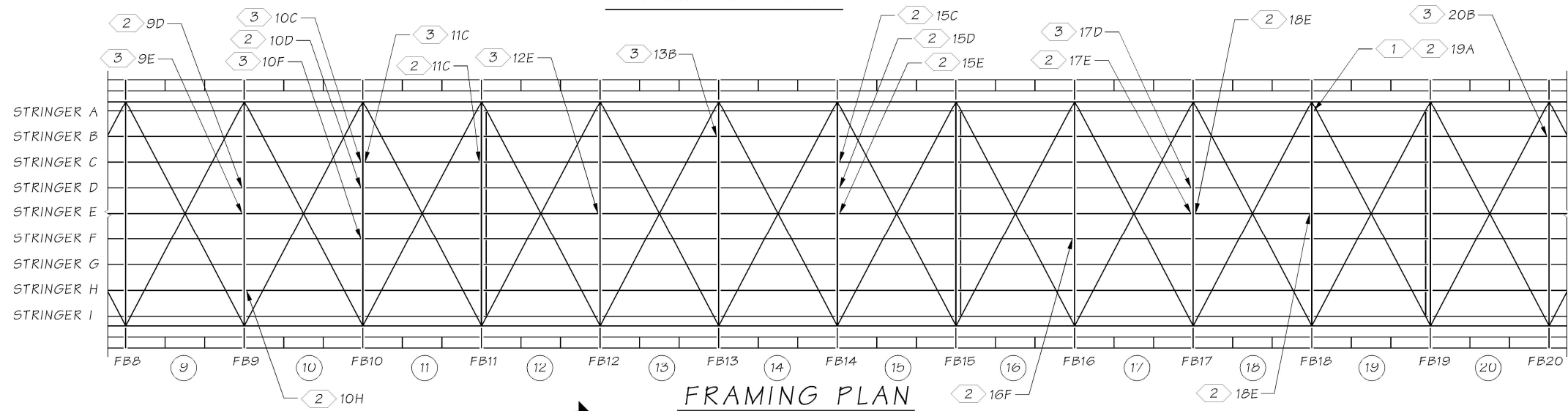
BRIDGE
SHEET
NO.
BB12

SHEET
69
OF
84
SHEETS

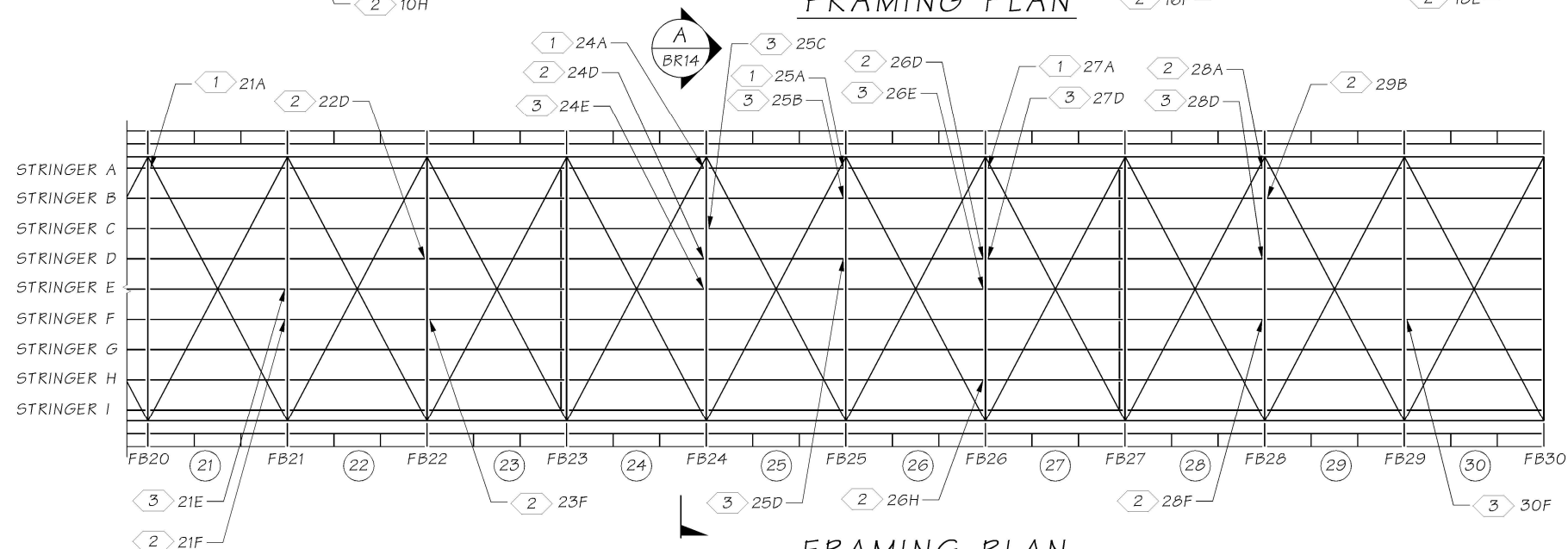
SR FILE NO. SHEET BB13



FRAMING PLAN

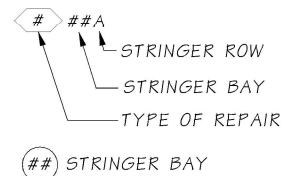


FRAMING PLAN



FRAMING PLAN

LEGEND:



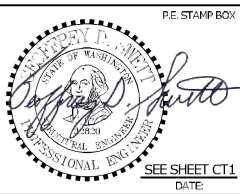
REPAIR TYPES:

- 1 TYPE 1 - ADD ANGLE
- 2 TYPE 2 - GRIND AND POLISH
- 3 TYPE 3 - FISH PLATE

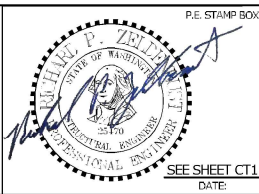
GENERAL AND STEEL NOTES

- ALL MATERIAL AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE WASHINGTON STATE DEPARTMENT OF TRANSPORTATION, STANDARD SPECIFICATIONS FOR ROAD, BRIDGE AND MUNICIPAL CONSTRUCTION, DATED 2023.
- THE DIMENSIONS SHOWN IN THE PLANS ARE BASED ON AS-BUILT PLANS. ALL RELEVANT DETAILS AND DIMENSIONS SHALL BE FIELD MEASURED AND VERIFIED BY THE CONTRACTOR PRIOR TO ORDERING MATERIALS AND PROCEEDING WITH CONSTRUCTION.
- THE CONTRACTOR SHALL SUBMIT A TYPE 2E WORKING DRAWING DETAILING METHOD FOR ACCESSING EACH REPAIR LOCATION INCLUDING ALL PROPOSED ATTACHMENT POINTS TO THE EXISTING BRIDGE. NO NEW HOLES SHALL BE DRILLED IN EXISTING MEMBERS WITHOUT PRIOR APPROVAL OF THE ENGINEER.
- ALL STEEL PLATES AND ANGLES SHALL BE ASTM A709 GR50 OR ASTM A572 GR 50.
- ALL FIELD CONNECTIONS SHALL BE MADE WITH HIGH STRENGTH BOLTS, WITH THE BOLT HEADS TOWARD THE OUTSIDE AND UNDERSIDE OF THE BRIDGE UNLESS OTHERWISE SHOWN IN THE PLANS. HIGH STRENGTH BOLTS SHALL BE GALVANIZED ASTM F3125 GRADE A325 TYPE 1 OR 3, 7/8 INCH DIAMETER, EXCEPT AS NOTED OTHERWISE. NUTS AND WASHERS SHALL CONFORM TO SECTION 9-06.5(3). THREADED RODS SHALL BE 3/4 INCH DIAMETER, GALVANIZED, AND SHALL CONFORM TO ASTM A449.
- ALL STRUCTURAL STEEL SHALL BE PAINTED IN ACCORDANCE WITH SECTION 6-07. WHEN SPECIFIED, GALVANIZING SHALL BE IN ACCORDANCE WITH AASHTO M 111, M 232, ASTM F2329, OR ASTM B695 CLASS 55 AS APPLICABLE. GALVANIZED BOLTS AND THREADED RODS TO BE PAINTED SHALL BE PREPARED IN ACCORDANCE WITH SECTION 6-07.3(11)A.

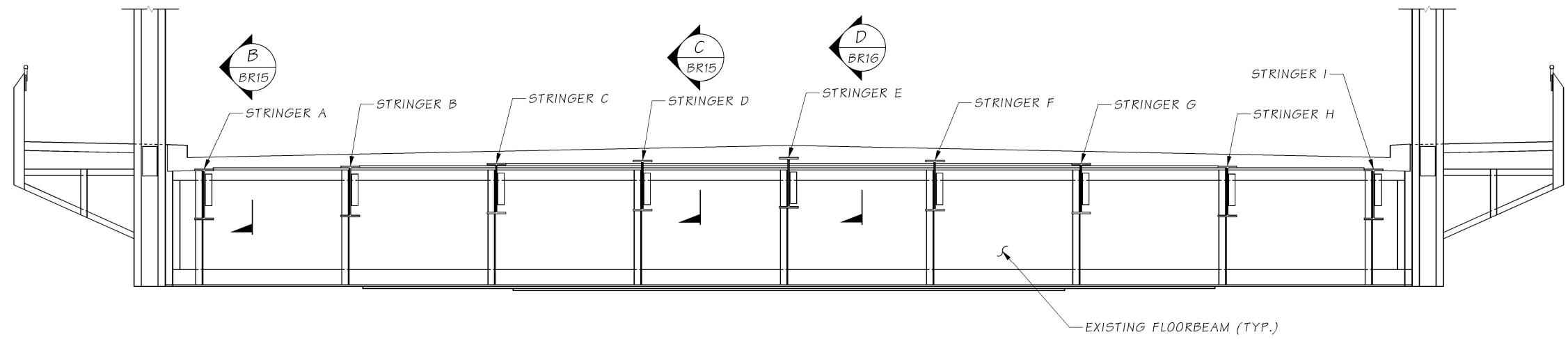
Bridge Design Engr.	Khaleghi, B	M:\Z-Team\XL6380 - 5.40w REPAIR>window files\LAYOUT 1.wnd				REGION NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
Supervisor	Zeldenrust, RP					10	WASH.			
Designed By	Swett, GD	11/22				JOB NUMBER				
Checked By	Martin, J	04/23				CONTRACT NO.				
Detailed By	DeBoer, J	11/22								
Bridge Projects Engr.										
Prelim. Plan By										
Architect/Specialist		DATE	REVISION	BY	APPD					



BRIDGE AND STRUCTURES OFFICE



I-5 NB/SB LEWIS R BR & NB TOUTLE R BR DECK REPAIR / OVERLAY BRIDGE NO. 5/40W		BRIDGE SHEET NO. BB13
FRAMING PLAN AND COPE CRACK REPAIR LOCATIONS		SHEET 70 OF 84 SHEETS



TYPICAL SECTION A
BR13

Bridge Design Engr.	Khaleghi, B	M:\Z-Team\XL6380 - 5.40w REPAIR>window files\TYP SECTION.wnd							
Supervisor	Zeldenrust, RP								
Designed By	Swett, GD	11/22							
Checked By	Martin, J	04/23							
Detailed By	DeBoer, J	11/22							
Bridge Projects Engr.									
Prelim. Plan By									
Architect/Specialist									
	DATE	REVISION	BY	APPD					

P.E. STAMP BOX

SEE SHEET CT1
DATE:

BRIDGE AND STRUCTURES OFFICE

P.E. STAMP BOX

SEE SHEET CT1
DATE:

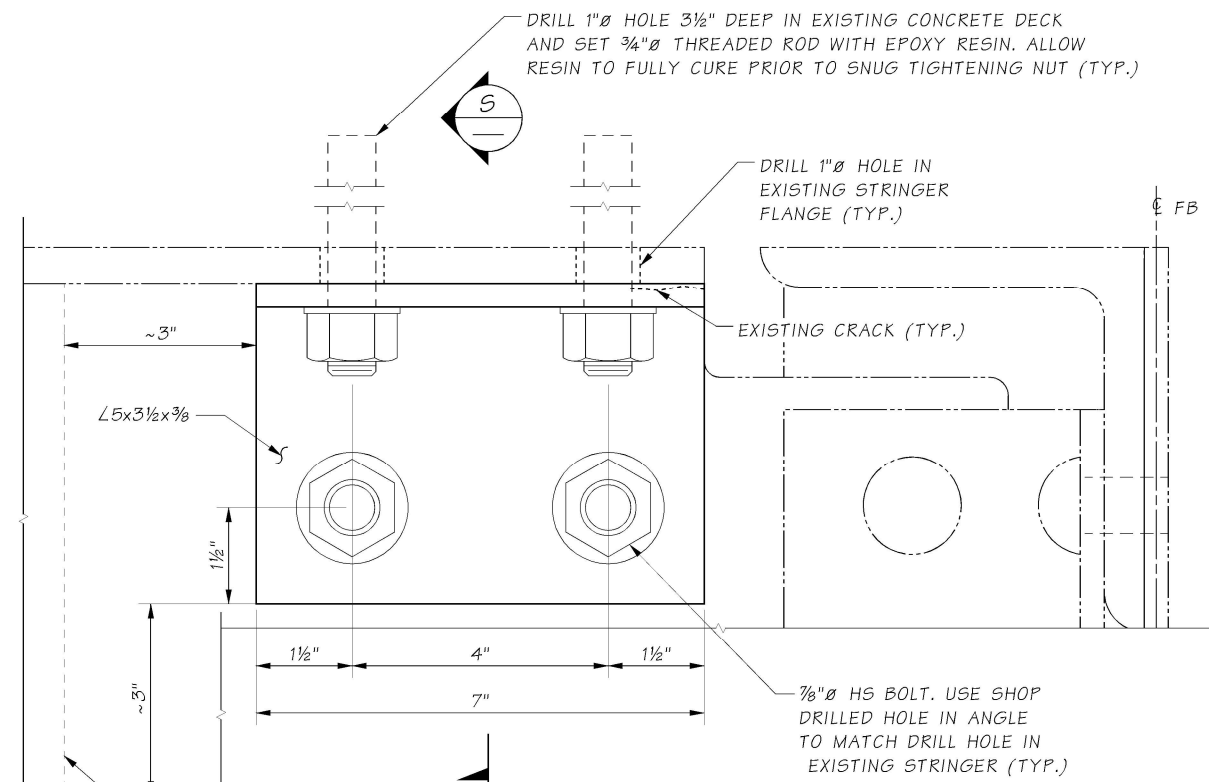
Washington State
Department of Transportation

I-5
NB/SB LEWIS R BR & NB TOUTLE R BR
DECK REPAIR / OVERLAY
BRIDGE NO. 5/40W

TYPICAL SECTION

BRIDGE SHEET NO.
BB14

SHEET
71
OF
84
SHEETS

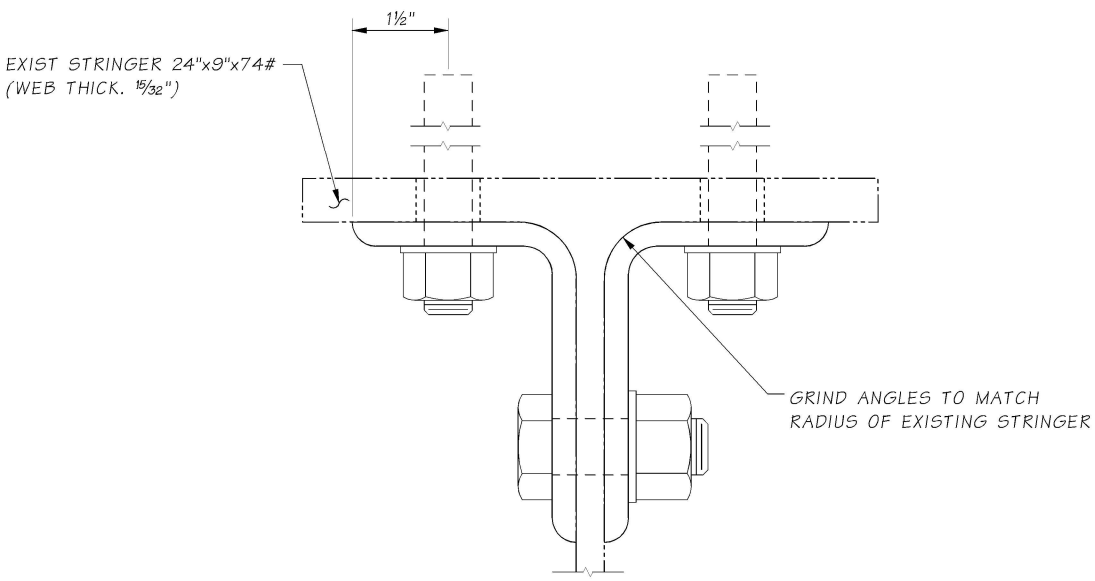


SECTION B
BR14

REPAIR DETAIL TYPE 1
ADD ANGLES

REPAIR LOCATION 24A SHOWN, OTHERS SIMILAR

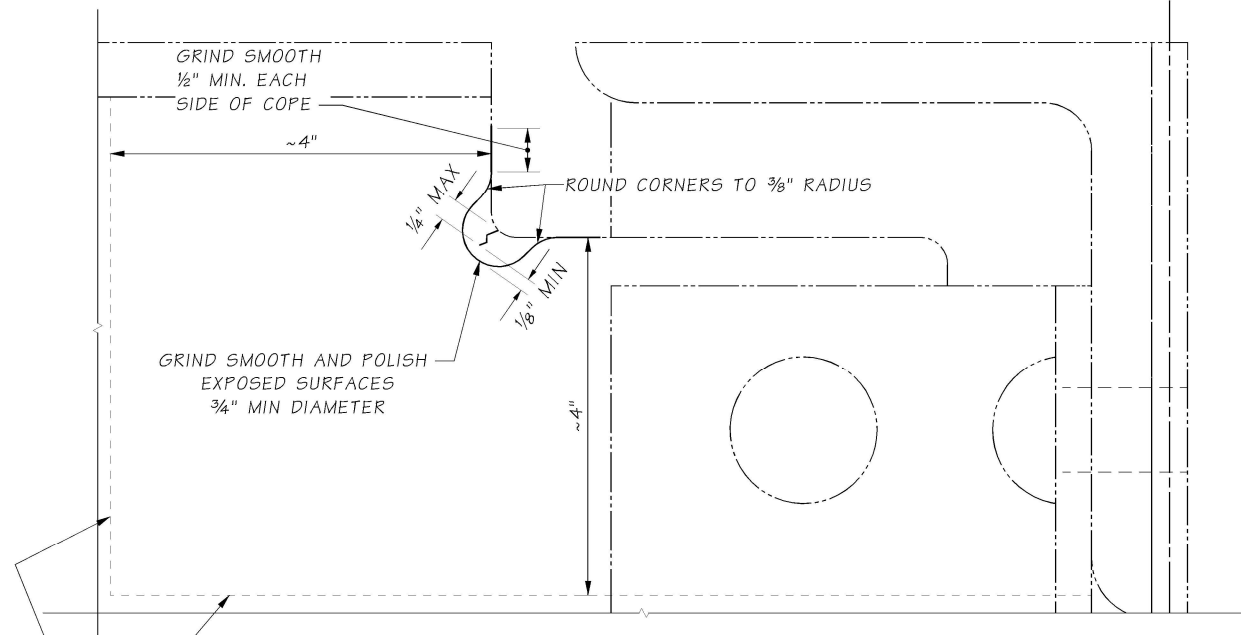
MIN. PAINT LIMITS
TYPICAL EACH
SIDE OF STRINGER



SECTION S

NOTE: CONTRACTOR MAY SUBSTITUTE A BENT PLATE FOR THE ANGLE

MIN. PAINT LIMITS
TYPICAL EACH
SIDE OF STRINGER



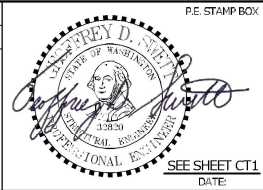
SECTION C
BR14

REPAIR DETAIL TYPE 2
GRIND AND POLISH

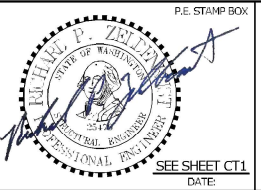
REPAIR LOCATION 24D SHOWN, OTHERS SIMILAR

- REMOVE EXISTING PAINT TO BARE METAL. PERFORM MAGNETIC PARTICLE EXAMINATION TO DETERMINE LOCATION AND END OF CRACK (CRACK TIP). IF CRACK LENGTH IS LONGER THAN 1/4", NOTIFY THE ENGINEER.
- GRIND OUT CRACK TO 3/4" MINIMUM DIAMETER PER DETAIL SHOWN (GRIND MIN. 1/8" BEYOND IDENTIFIED CRACK TIP)
- GRIND SMOOTH ALL SURFACES SHOWN TO 32 RMS FINISH
- REPAIR PAINT

Bridge Design Engr.	Khaleghi, B	M:\Z-Team\XL6380 - 5.40w REPAIR\window files\DETAILS 1.wnd					REGION NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
Supervisor	Zeldenrust, RP						10	WASH.			
Designed By	Swett, GD	11/22					JOB NUMBER				
Checked By	Martin, J	04/23					CONTRACT NO.				
Detailed By	DeBoer, J	11/22									
Bridge Projects Engr.											
Prelim. Plan By											
Architect/Specialist		DATE	REVISION	BY	APPD						

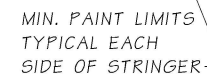


BRIDGE AND STRUCTURES OFFICE



I-5
NB/SB LEWIS R BR & NB TOUTLE R BR
DECK REPAIR / OVERLAY
BRIDGE NO. 5/40W
DETAILS
1 OF 2

BRIDGE SHEET NO.
BB15
SHEET 72 OF 84 SHEETS



SECTION D
BR14

REPAIR DETAIL TYPE 3

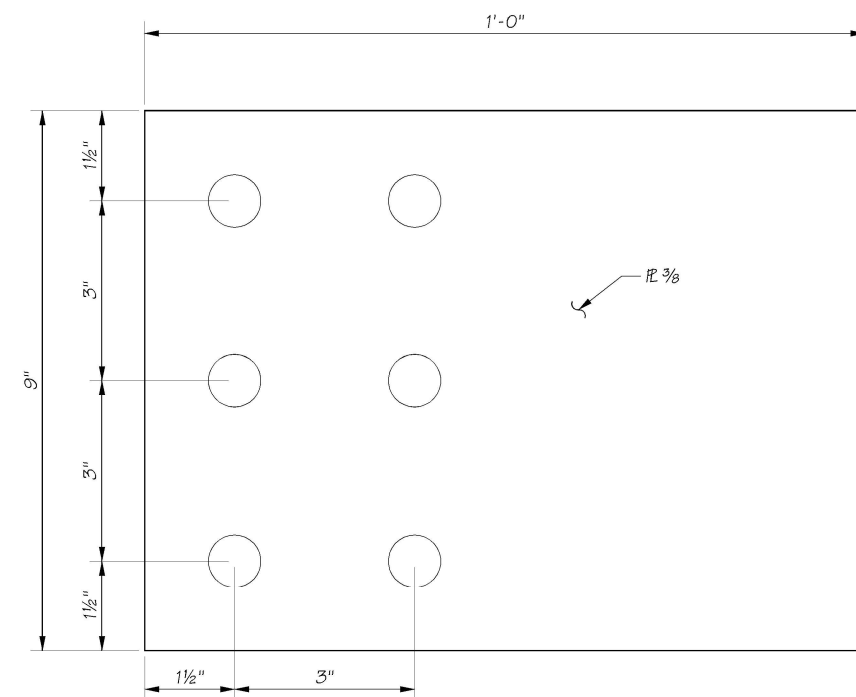
FISH PLATE

REPAIR LOCATION 24E SHOWN, OTHERS SIMILAR.

REPAIR NOTES

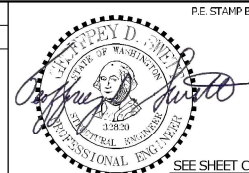
INSTALL FISHPLATE

- REMOVE EXISTING PAINT AND PERFORM MAGNETIC PARTICLE INSPECTION TO LOCATE END OF CRACK (CRACK TIP).
- INSTALL STOPCRACKEX BUSHING AT $\frac{1}{16}$ " FROM END OF CRACK PER MANUFACTURER'S RECOMMENDATIONS.
- SURFACE PREPARE AND PRIME PAINT ALL BARE STEEL SURFACES TO BE COVERED BY FISH PLATE.
- RESTRICT TRAFFIC FROM LANE IMMEDIATELY ABOVE STRINGER TO BE REPAIRED.
- REPLACE BOTTOM TWO RIVETS IN SUPPORT ANGLE WITH 1"Ø HS BOLTS. REAM EXIST. BOLT HOLE TO $1\frac{1}{16}$ "Ø.
- REMOVE TOP THREE RIVETS.
- USE SHOP DRILLED HOLES IN FISH PLATE TO MATCH DRILL HOLES IN EXISTING STRINGER AND EXISTING RIVET HOLES TO MATCH DRILL HOLES IN FISH PLATE. BOLT HOLES SHALL BE $\frac{1}{16}$ "Ø.
- INSTALL FISH PLATES, FILL PLATES AND HIGH STRENGTH BOLTS
- PAINT ALL STEEL SURFACES TO THE LIMITS SHOWN IN THE DETAIL.

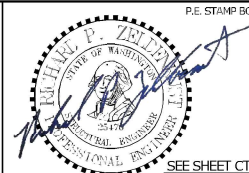


FISH PLATE DETAIL

Bridge Design Engr.		Khaloghi, B		M:\Z-Team\XL6380 - 5.40w REPAIR\window files\DETAILS 2.wnd									
Supervisor		Zeldenrust, RP						REGION NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS	
Designed By		Swett, GD 11/22						10	WASH.				
Checked By		Martin, J 04/23											
Detailed By		DeBoer, J 11/22											
Bridge Projects Engr.								JOB NUMBER					
Prelim. Plan By								CONTRACT NO.					
Architect/Specialist		DATE		REVISION		BY		APPD					



BRIDGE
AND
STRUCTURES
OFFICE



**Washington State
Department of Transportation**

I-5
NB/SB LEWIS R BR & NB TOUTLE R BR
DECK REPAIR / OVERLAY
BRIDGE NO. 5/40W

DETAILS
2 OF 2

BRIDGE SHEET NO.	316
SHEET 73 OF 34 SHEETS	



STRINGER 21A AT FB20

1



STRINGER 24A AT FB24

1



STRINGER 25A AT FB25

1



STRINGER 27A AT FB26

1



STRINGER 19A AT FB18

1

2

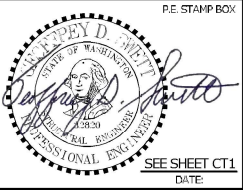
REPAIR TYPES:

- 1 TYPE 1 - ADD ANGLE
2 TYPE 2 - GRIND AND POLISH
3 TYPE 3 - FISH PLATE

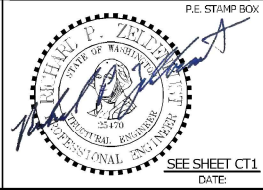
SR FILE NO. SHEET 8517

Bridge Design Engr.	Khaleghi, B	M:\Z-Team\XL6380 - 5.40w REPAIR>window files\PICS 1.wnd						REGION NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
Supervisor	Zeldenrust, RP							10	WASH.			
Designed By	Swett, GD	12/22						JOB NUMBER				
Checked By	Martin, J	04/23						CONTRACT NO.				
Detailed By	DeBoer, J	12/22										
Bridge Projects Engr.												
Prelim. Plan By												
Architect/Specialist		DATE	REVISION	BY	APPD							

Tue Apr 18 13:44:25 2023



P.E. STAMP BOX
BRIDGE AND STRUCTURES OFFICE



I-5
NB/SB LEWIS R BR & NB TOUTLE R BR
DECK REPAIR / OVERLAY
BRIDGE NO. 5/40W
SITE PHOTOS
1 OF 8

BRIDGE SHEET NO.
BB17
SHEET 74 OF 84
SHEETS



STRINGER 1D AT FB1

2



STRINGER 5E AT FB5

2



STRINGER 6D AT FB5

2



STRINGER 9D AT FB9

2



STRINGER 10H AT FB9

2



STRINGER 10D AT FB10

2

REPAIR TYPES:

- 1 TYPE 1 - ADD ANGLE
2 TYPE 2 - GRIND AND POLISH
3 TYPE 3 - FISH PLATE

Bridge Design Engr.	Khaleghi, B	M:\Z-Team\XL6380 - 5.40w REPAIR>window files\PICS 2.wnd						REGION NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
Supervisor	Zeldenrust, RP							10	WASH.			
Designed By	Swett, GD	12/22						JOB NUMBER				
Checked By	Martin, J	04/23						CONTRACT NO.				
Detailed By	DeBoer, J	12/22										
Bridge Projects Engr.												
Prelim. Plan By												
Architect/Specialist		DATE	REVISION	BY	APPD							

P.E. STAMP BOX

SEE SHEET CT1
DATE:

BRIDGE AND STRUCTURES OFFICE

P.E. STAMP BOX

SEE SHEET CT1
DATE:

Washington State
Department of Transportation

I-5
NB/SB LEWIS R BR & NB TOUTLE R BR
DECK REPAIR / OVERLAY
BRIDGE NO. 5/40W

SITE PHOTOS
2 OF 8

BRIDGE SHEET NO.
BB18

SHEET
75
OF
84
SHEETS



STRINGER 11C AT FB11

2



STRINGER 15C AT FB14

2



STRINGER 15D AT FB14

2



STRINGER 15E AT FB14

2



STRINGER 16F AT FB16

2



STRINGER 17E AT FB17

2

REPAIR TYPES:

- 1 TYPE 1 - ADD ANGLE
2 TYPE 2 - GRIND AND POLISH
3 TYPE 3 - FISH PLATE

Bridge Design Engr.	Khaleghi, B	M:\Z-Team\XL6380 - 5.40w REPAIR>window files\PICS 3.wnd					
Supervisor	Zeldenrust, RP						
Designed By	Swett, GD	12/22					
Checked By	Martin, J	04/23					
Detailed By	DeBoer, J	12/22					
Bridge Projects Engr.							
Prelim. Plan By							
Architect/Specialist		DATE	REVISION	BY	APPD		

 SEE SHEET CT1 DATE:	BRIDGE AND STRUCTURES OFFICE	 SEE SHEET CT1 DATE:	 Washington State Department of Transportation	I-5 NB/SB LEWIS R BR & NB TOUTLE R BR DECK REPAIR / OVERLAY BRIDGE NO. 5/40W SITE PHOTOS 3 OF 8	BRIDGE SHEET NO. BB19 SHEET 76 OF 84 SHEETS
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STRINGER 18E AT FB17

2



STRINGER 18E AT FB18

2



STRINGER 22D AT FB22

2



STRINGER 23F AT FB22

2



STRINGER 24D AT FB24

2



STRINGER 26D AT FB26

2

REPAIR TYPES:

- 1 TYPE 1 - ADD ANGLE
- 2 TYPE 2 - GRIND AND POLISH
- 3 TYPE 3 - FISH PLATE

SR FILE NO. SHEET BB20

Bridge Design Engr.	Khaleghi, B	M:\Z-Team\XL6380 - 5.40w REPAIR>window files\PICS 4.wnd										
Supervisor	Zeldenrust, RP					REGION NO.	STATE	FED. AID PROJ. NO.		SHEET NO.	TOTAL SHEETS	
Designed By	Swett, GD	12/22				10	WASH.					
Checked By	Martin, J	04/23										
Detailed By	DeBoer, J	12/22										
Bridge Projects Engr.						JOB NUMBER						
Prelim. Plan By						CONTRACT NO.						
Architect/Specialist		DATE	REVISION		BY	APPD						

P.E. STAMP BOX

SEE SHEET CT1
DATE:

BRIDGE AND STRUCTURES OFFICE

P.E. STAMP BOX

SEE SHEET CT1
DATE:

Washington State
Department of Transportation

I-5
NB/SB LEWIS R BR & NB TOUTLE R BR
DECK REPAIR / OVERLAY
BRIDGE NO. 5/40W

SITE PHOTOS
4 OF 8

BRIDGE SHEET NO.
BB20

SHEET
77
OF
84
SHEETS



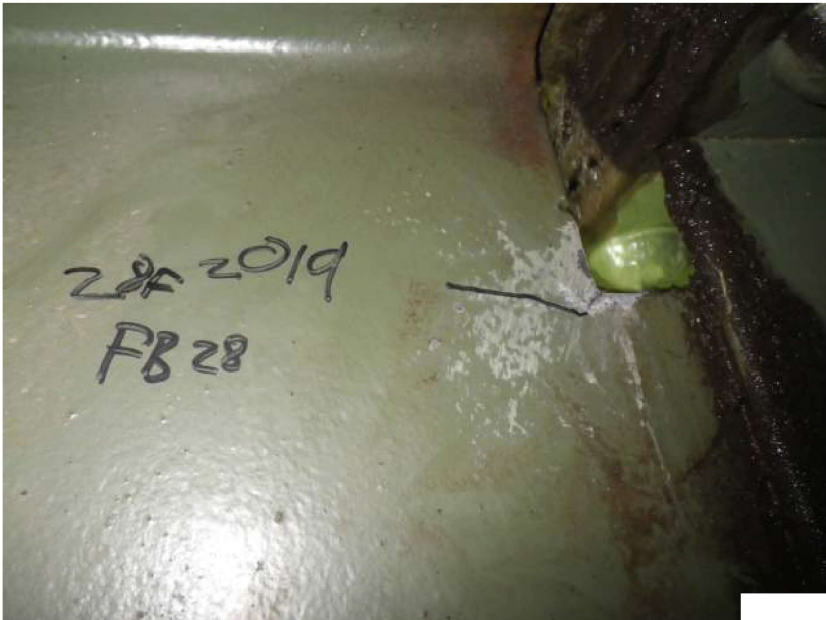
STRINGER 26H AT FB26

2



STRINGER 28A AT FB28

2



STRINGER 28F AT FB28

2



STRINGER 29B AT FB29

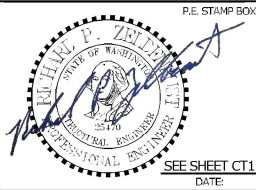
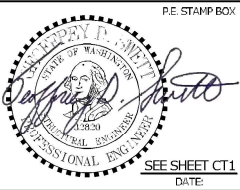
2

REPAIR TYPES:

- 1 TYPE 1 - ADD ANGLE
- 2 TYPE 2 - GRIND AND POLISH
- 3 TYPE 3 - FISH PLATE

SR FILE NO. SHEET 8521

Bridge Design Engr.	Khaleghi, B	M:\Z-Team\XL6380 - 5.40w REPAIR>window files\PICS 5.wnd						REGION NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
Supervisor	Zeldenrust, RP							10	WASH.			
Designed By	Swett, GD	12/22						JOB NUMBER				
Checked By	Martin, J	04/23						CONTRACT NO.				
Detailed By	DeBoer, J	12/22										
Bridge Projects Engr.												
Prelim. Plan By												
Architect/Specialist		DATE	REVISION	BY	APPD							



I-5 NB/SB LEWIS R BR & NB TOUTLE R BR DECK REPAIR / OVERLAY BRIDGE NO. 5/40W	
SITE PHOTOS 5 OF 8	

BRIDGE SHEET NO.	BB21
SHEET	78
OF	84
SHEETS	



STRINGER 2C AT FB1

3



STRINGER 9E AT FB9

3



STRINGER 10C AT FB10

3



STRINGER 10F AT FB10

3



STRINGER 11C AT FB10

3



STRINGER 12E AT FB12

3

REPAIR TYPES:

- 1 TYPE 1 - ADD ANGLE
- 2 TYPE 2 - GRIND AND POLISH
- 3 TYPE 3 - FISH PLATE

Bridge Design Engr.	Khaleghi, B	M:\Z-Team\XL6380 - 5.40w REPAIR>window files\PICS 6.wnd					REGION NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
Supervisor	Zeldenrust, RP					10	WASH.				
Designed By	Swett, GD	12/22				JOB NUMBER					
Checked By	Martin, J	04/23				CONTRACT NO.					
Detailed By	DeBoer, J	12/22									
Bridge Projects Engr.											
Prelim. Plan By											
Architect/Specialist		DATE	REVISION	BY	APPD						

P.E. STAMP BOX

BRIDGE AND STRUCTURES OFFICE

P.E. STAMP BOX

SEE SHEET CT1

Washington State Department of Transportation

I-5 NB/SB LEWIS R BR & NB TOUTLE R BR DECK REPAIR / OVERLAY

BRIDGE NO. 5/40W

SITE PHOTOS 6 OF 8

BRIDGE SHEET NO.

BB22

SHEET 79 OF 84 SHEETS



STRINGER 13B AT FB13

3



STRINGER 17D AT FB17

3



STRINGER 20B AT FB20

3



STRINGER 21E AT FB21

3



STRINGER 24E AT FB24

3



STRINGER 25C AT FB24

3

REPAIR TYPES:

- 1 TYPE 1 - ADD ANGLE
- 2 TYPE 2 - GRIND AND POLISH
- 3 TYPE 3 - FISH PLATE

Bridge Design Engr.	Khaleghi, B	M:\Z-Team\XL6380 - 5.40w REPAIR>window files\PICS 7.wnd					
Supervisor	Zeldenrust, RP						
Designed By	Swett, GD	12/22					
Checked By	Martin, J	04/23					
Detailed By	DeBoer, J	12/22					
Bridge Projects Engr.							
Prelim. Plan By							
Architect/Specialist							
	DATE	REVISION	BY	APPD			

P.E. STAMP BOX

SEE SHEET CT1
DATE:

BRIDGE AND STRUCTURES OFFICE

P.E. STAMP BOX

SEE SHEET CT1
DATE:

Washington State
Department of Transportation

I-5
NB/SB LEWIS R BR & NB TOUTLE R BR
DECK REPAIR / OVERLAY
BRIDGE NO. 5/40W

SITE PHOTOS
7 OF 8

BRIDGE SHEET NO.
BB23

SHEET
80
OF
84
SHEETS



STRINGER 25B AT FB25

3



STRINGER 25D AT FB25

3



STRINGER 26E AT FB26

3



STRINGER 27D AT FB26

3



STRINGER 28D AT FB28

3



STRINGER 30F AT FB29

3

REPAIR TYPES:

- 1 TYPE 1 - ADD ANGLE
- 2 TYPE 2 - GRIND AND POLISH
- 3 TYPE 3 - FISH PLATE

Bridge Design Engr.	Khaleghi, B	M:\Z-Team\XL6380 - 5.40w REPAIR>window files\PICS 8.wnd						REGION NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
Supervisor	Zeldenrust, RP							10	WASH.			
Designed By	Swett, GD	12/22						JOB NUMBER				
Checked By	Martin, J	04/23						CONTRACT NO.				
Detailed By	DeBoer, J	12/22										
Bridge Projects Engr.												
Prelim. Plan By												
Architect/Specialist		DATE	REVISION	BY	APPD							

P.E. STAMP BOX

SEE SHEET CT1

BRIDGE AND STRUCTURES OFFICE

P.E. STAMP BOX

SEE SHEET CT1

Washington State
Department of Transportation

I-5
NB/SB LEWIS R BR & NB TOUTLE R BR
DECK REPAIR / OVERLAY
BRIDGE NO. 5/40W

SITE PHOTOS
8 OF 8

BRIDGE SHEET NO.

BB24

SHEET 81 OF 84 SHEETS

MINIMUM LANE CLOSURE TAPER LENGTH = L (feet)										
LANE WIDTH (feet)	Posted Speed (mph)									
	25	30	35	40	45	50	55	60	65	70
10	105	150	205	270	450	500	550	-	-	-
11	115	165	225	295	495	550	605	660	-	-
12	125	180	245	320	540	600	660	720	780	840

MINIMUM SHOULDER TAPER LENGTH = L/3 (feet)										
SHOULDER WIDTH (feet)	Posted Speed (mph)									
	25	30	35	40	45	50	55	60	65	70
8'	40	40	60	90	120	130	150	160	170	190
10'	40	60	90	90	150	170	190	200	220	240

USE A MINIMUM 3 DEVICES TAPER FOR SHOULDER LESS THEN 8'.

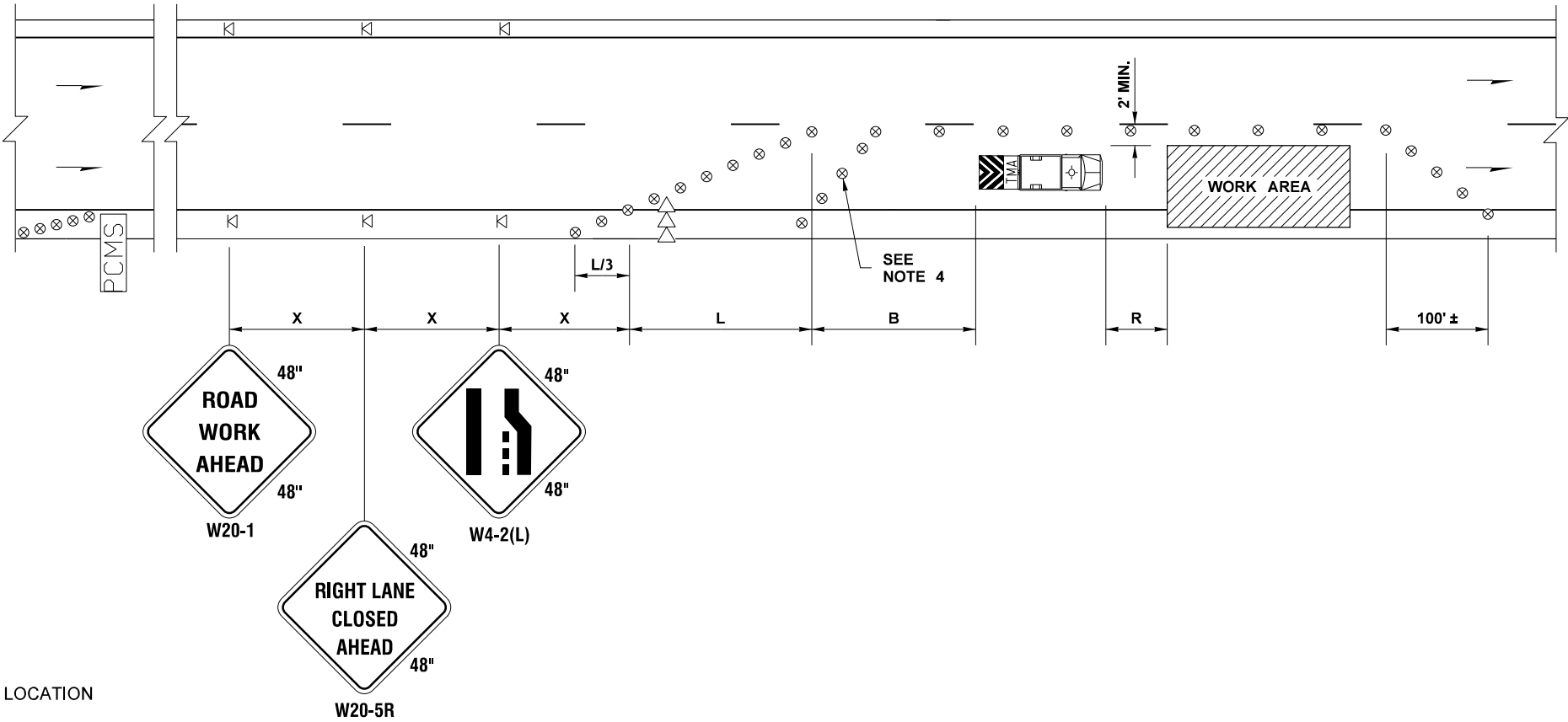
SIGN SPACING = X (1)		
FREEWAYS & EXPRESSWAYS	55 / 70 MPH	1500' ±
RURAL HIGHWAYS	60 / 65 MPH	800' ±
RURAL ROADS	45 / 55 MPH	500' ±
RURAL ROADS & URBAN ARTERIALS	35 / 40 MPH	350' ±
RURAL ROADS & URBAN ARTERIALS	25 / 30 MPH	200' ± (2)
RESIDENTIAL & BUSINESS DISTRICTS		
URBAN STREETS	25 MPH OR LESS	100' ± (2)

(1) ALL SPACING MAY BE ADJUSTED TO ACCOMMODATE INTERCHANGE RAMP, AT-GRADE INTERSECTIONS AND DRIVEWAYS.
(2) THIS SPACING MAY BE REDUCED IN URBAN AREAS TO FIT ROADWAY CONDITIONS.

CHANNELIZATION DEVICE SPACING (feet)		
MPH	TAPER	TANGENT
50/70	40	80
35/45	30	60
25/30	20	40

BUFFER DATA										
LONGITUDINAL BUFFER SPACE = B										
SPEED (MPH)	25	30	35	40	45	50	55	60	65	70
LENGTH (feet)	155	200	250	305	360	425	495	570	645	730

TRANSPORTABLE ATTENUATOR ROLL AHEAD DISTANCE = R					
HOST VEHICLE WEIGHT 9,900 TO 22,000 lbs.			HOST VEHICLE WEIGHT > 22,000 lbs.		
< 45 MPH	45-55 MPH	> 55 MPH	< 45 MPH	45-55 MPH	> 55 MPH
100'	123'	172'	74'	100'	150'



PCMS	
1	2
RIGHT LANE CLOSURE	1 MILE AHEAD
2.0 SEC	2.0 SEC

FIELD LOCATE 1 MILE ± IN ADVANCE OF LANE CLOSURE SIGNING.

- LEGEND
- ⊗

TEMPORARY SIGN LOCATION
- ⊗

TRAFFIC SAFETY DRUM
- ⇨⇨

SEQUENTIAL ARROW SIGN
- TRANSPORTABLE ATTENUATOR
- PORTABLE CHANGEABLE MESSAGE SIGN

SINGLE-LANE CLOSURE FOR MULTI-LANE ROADWAYS

- NOTES
1. SEE SPECIAL PROVISIONS FOR WORK HOUR RESTRICTIONS.

2. EXTEND DEVICE TAPER AT L/3 ACROSS SHOULDER.

3. DEVICES SHALL NOT ENCROACH INTO THE ADJACENT LANE.

4. USE TRANSVERSE DEVICES IN CLOSED LANE EVERY 1000' (FT) (RECOMMENDED).

5. DEVICE SPACING FOR THE DOWNSTREAM TAPER SHALL BE 20' (FT).

6. ALL SIGNS ARE BLACK ON ORANGE.

NOT TO SCALE

FILE NAME		\\Wsdot\loc\sw\Group\Engineering\444301\05-Design\02-Projects\I-5 NB & SB Lewis River Br and NB Toutle River Br Deck Repair\1-CADD-Plans\ContractPlans\22X333_PS_TCP.dgn		REGION NO.		STATE		FED.AID PROJ.NO.		Plot 1	
TIME		10:01:49 AM		10		WASH		NHFP-0051(316)		PLAN REF NO	
DATE		3/14/2023								TC1	
PLOTTED BY		hillb									
DESIGNED BY		T. PANKAU									
ENTERED BY		CCEO									
CHECKED BY		A. ROMANCHOCK									
PROJ. ENGR.		S. FELL									
REGIONAL ADM.		C. FRANCIS									
		REVISION		DATE		BY		LOCATION NO.		SHEET	
										82	
										OF	
										84	
										SHEETS	

MINIMUM LANE CLOSURE TAPER LENGTH = L (feet)										
LANE WIDTH (feet)	Posted Speed (mph)									
	25	30	35	40	45	50	55	60	65	70
10	105	150	205	270	450	500	550	-	-	-
11	115	165	225	295	495	550	605	660	-	-
12	125	180	245	320	540	600	660	720	780	840

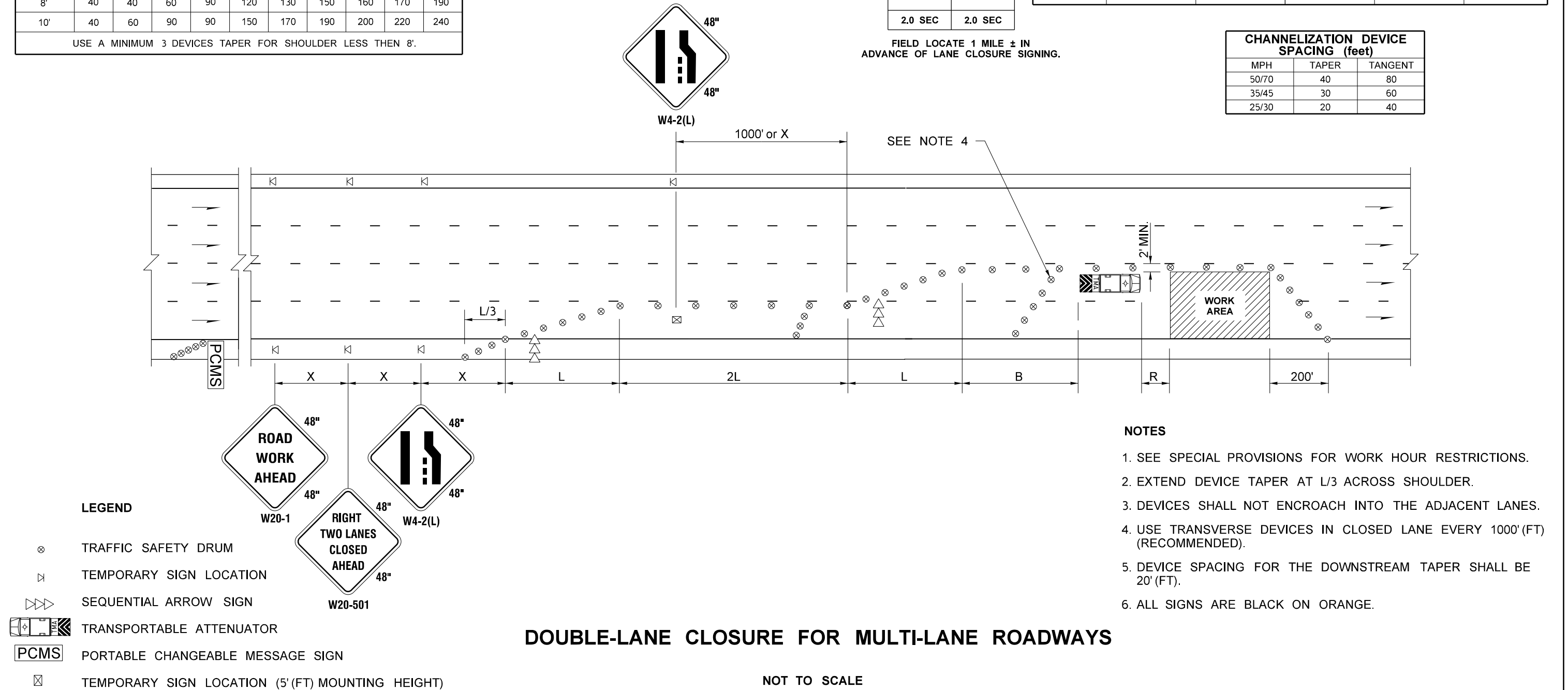
MINIMUM SHOULDER TAPER LENGTH = L/3 (feet)										
SHOULDER WIDTH (feet)	Posted Speed (mph)									
	25	30	35	40	45	50	55	60	65	70
8'	40	40	60	90	120	130	150	160	170	190
10'	40	60	90	90	150	170	190	200	220	240
USE A MINIMUM 3 DEVICES TAPER FOR SHOULDER LESS THEN 8'.										

SIGN SPACING = X (1)		
FREEWAYS & EXPRESSWAYS	55 / 70 MPH	1500' ±
RURAL HIGHWAYS	60 / 65 MPH	800' ±
RURAL ROADS	45 / 55 MPH	500' ±
RURAL ROADS & URBAN ARTERIALS	35 / 40 MPH	350' ±
RURAL ROADS & URBAN ARTERIALS RESIDENTIAL & BUSINESS DISTRICTS	25 / 30 MPH	200' ± (2)
URBAN STREETS	25 MPH OR LESS	100' ± (2)
(1) ALL SPACING MAY BE ADJUSTED TO ACCOMMODATE INTERCHANGE RAMP, AT-GRADE INTERSECTIONS AND DRIVEWAYS. (2) THIS SPACING MAY BE REDUCED IN URBAN AREAS TO FIT ROADWAY CONDITIONS.		

PCMS	
1	2
2 LANES CLOSED AHEAD	WATCH FOR SLOW TRAFFIC
2.0 SEC	2.0 SEC


BUFFER DATA										
LONGITUDINAL BUFFER SPACE = B										
SPEED (MPH)	25	30	35	40	45	50	55	60	65	70
LENGTH (feet)	155	200	250	305	360	425	495	570	645	730
TRANSPORTABLE ATTENUATOR ROLL AHEAD DISTANCE = R										
HOST VEHICLE WEIGHT 9,900 TO 22,000 lbs.						HOST VEHICLE WEIGHT > 22,000 lbs.				
< 45 MPH	45-55 MPH	> 55 MPH		< 45 MPH	45-55 MPH	> 55 MPH				
100'	123'	172'		74'	100'	150'				

CHANNELIZATION DEVICE SPACING (feet)		
MPH	TAPER	TANGENT
50/70	40	80
35/45	30	60
25/30	20	40



DOUBLE-LANE CLOSURE FOR MULTI-LANE ROADWAYS

NOT TO SCALE

FILE NAME \\Wsdot\loc\sw\Group\Engineering\444301\05-Design\02-Projects\I-5 NB & SB Lewis River Br and NB Toutle River Br Deck Repair\1-CADD-Plans\ContractPlans\22X333_PS_TCP.dgn																				 Washington State Department of Transportation										I-5 NB AND SB LEWIS RVR BR DECK REPAIR / OVERLAY TRAFFIC CONTROL PLAN										Plot 2	
TIME 10:01:50 AM								REGION NO. 10		STATE WASH		FED.AID PROJ.NO. NHFP-0051(316)																		PLAN REF NO TC2											
DATE 3/14/2023																																									
PLOTTED BY hillb																																									
DESIGNED BY T. PANKAU																																									
ENTERED BY CCEO																																									
CHECKED BY A. ROMANCHOCK																																									
PROJ. ENGR. S. FELL																																									
REGIONAL ADM. C. FRANCIS																																									
REVISION										DATE		BY																						SHEET 83 OF 84 SHEETS							

